



Agenda

Planning Review Meeting

Wednesday 9th December 2015 at 7:00pm

Queenscliff Town Hall
50 Learmonth Street, Queenscliff

Distribution

Councillors

Cr. Helene Cameron (Mayor)
Cr. Greg Jones (Deputy Mayor)
Cr. Bob Merriman
Cr. Susan Salter
Cr. Sue Wasterval

Officers

Lenny Jenner, Chief Executive Officer
Phil Josipovic, General Manager Planning & Infrastructure
Leah Protyniak, Senior Planner

In accordance with the Borough of Queenscliffe Local Law No 1, 2010, the information contained within this Agenda is for the confidential and privileged use of Councillors until at least 48 hours prior to this meeting

THIS MATERIAL DOES NOT NECESSARILY REFLECT THE VIEWS OF COUNCIL



Planning Review Meeting

A guide to understanding meeting protocol

There is a need to cover some simple protocols as each meeting will often involve people attending for the first time.

1. Planning Review meetings are held to provide additional information to Councillors in preparation for the following formal council meeting. The meetings are informal and proponents and submitters to any planning matter are encouraged to address council.
 2. This is not a debating forum – we are trying to obtain the best possible understanding of the matter.
 3. We ask that parties addressing Council speak to the chair and not involve the gallery.
 4. Submitters are asked to elaborate on their written submissions – not just read out their letter/email – all councillors have a copy of written material.
 5. The meeting process will typically adopt the following sequence:
 - Introduction and welcome by the Chairperson.
 - Overview presentation by Council's Planning Officer.
 - The Applicant is given 5-10 minutes to outline their proposal – longer time may be given at the discretion of the chair depending on the complexity of the matter.
 - We ask submitters to limit their comments to 5 minutes bearing in mind we are seeking elaboration on the comments already received in their submission.
 - Following the last submitter the Applicant will be given an opportunity to clarify any matter of fact – but not to comment on matters of opinion.
 - Throughout this process Councillors will be able to ask questions of the Applicant, submitters or a Council Officer.
-



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1. OPENING OF MEETING

2. APOLOGIES

3. PECUNIARY INTEREST & CONFLICT OF INTEREST DISCLOSURES

Councillors:

Officers:



4. PLANNING & DEVELOPMENT

4.1 57-59 Point Lonsdale Road, Point Lonsdale

Planning Permit number: 2015/027

SUMMARY

Proposal	Buildings and works associated with the construction of two buildings (three storey) comprising a shop and four dwellings, reduction of the standard car parking requirement of clause 52.06 and waiver of loading bay requirement of Clause 52.07 Application and plans: Refer <i>Appendix 1</i>
Zone/Overlays	Commercial 1 Zone (C1Z) Design and Development Overlay - Schedule 2 (DDO2) Vegetation Protection Overlay – Schedule 1
Permit Triggers	C1Z – Clause 34.01: Buildings and works DDO2 – Clause 43.02: Buildings and works Clause 52.06: car parking Clause 52.07: loading bay requirement
Public Notification	Advertised by registered post to adjoining property owners and occupiers, two (2) signs on site, a notice in The Echo newspaper and notice in municipal offices for 14 days.
Submissions	Six (6) submissions received Copies of submissions provided to Councillors: Refer <i>Confidential Appendix 2</i>
Key issues raised by submitters	Overlooking, three storey building, neighbourhood character, overshadowing and impact on daylight to existing windows, car parking, setbacks, impact on future development on neighbouring sites, overdevelopment, height, materials, landscaping.



4.1.1. Applicant to present to Council

4.1.2. Submitters to present to Council

4.1.3. Applicant to readdress Council



5. APPENDIX 1- APPLICATION DOCUMENTS 57-59 Point Lonsdale Road, Point Lonsdale



Planning Enquiries
Phone:
Web: <http://www.queenscliffe.vic.gov.au>

Office Use Only

Application No.: 2015/027

Date Lodged: RECEIVED

Application for Planning Permit

If you need help to complete this form, read [How to complete the Application for Planning Permit form](#).

Any material submitted with this application, including plans and personal information, will be made available for public viewing, including electronically, and copies may be made for interested parties for the purpose of enabling consideration and review as part of a planning process under the *Planning and Environment Act 1987*. If you have any concerns, please contact Council's planning department.

Questions marked with an asterisk (*) are mandatory and must be completed.

If the space provided on the form is insufficient, attach a separate sheet.

Clear Form

The Land

- 1 Address of the land. Complete the Street Address and one of the Formal Land Descriptions.

Street Address *

Unit No.:	St. No.: 57-59	St. Name: Point Lonsdale Road
Suburb/Locality: Point Lonsdale		Postcode: 3225

Formal Land Description *

Complete either A or B.

This information can be found on the certificate of title.

A ☐ Lot No.: 1 ☐ Lodged Plan ☐ Title Plan ☒ Plan of Subdivision No.: 3225

OR

B ☐ Crown Allotment No.: Section No.:

Parish/Township Name:

The Proposal

- You must give full details of your proposal and attach the information required to assess the application. Insufficient or unclear information will delay your application.

- 2 For what use, development or other matter do you require a permit? *

If you need help about the proposal, read:
[How to Complete the Application for Planning Permit Form](#)

Use and development of the land for a retail premises and dwellings including a reduction in the car parking provision and a waiver of loading, generally in accordance with the submitted plans and supporting information.

☒ Provide additional information on the proposal, including: plans and elevations; any information required by the planning scheme, requested by Council or outlined in a Council planning permit checklist; and if required, a description of the likely effect of the proposal.

- 3 Estimated cost of development for which the permit is required *

Cost \$1.9 million

You may be required to verify this estimate.

Insert '0' if no development is proposed (eg. change of use, subdivision, removal of covenant, liquor licence)

Existing Conditions

- 4 Describe how the land is used and developed now *
- eg. vacant, three dwellings, medical centre with two practitioners, licensed restaurant with 80 seats, grazing.

Two storey building that contains retail at ground level and a dwelling at first floor level.

☒ Provide a plan of the existing conditions. Photos are also helpful.



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Town Planners

Town Planning and Urban Context Report

57-59 Point Lonsdale Road, Point Lonsdale

March 2015

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1 Introduction

This report has been prepared to accompany a planning permit application to the Borough of Queenscliffe for development of the land at 57-59 Point Lonsdale Road, Point Lonsdale.

The application proposes the construction of two buildings comprising a total of four dwellings including two apartments over a retail tenancy at ground level fronting Point Lonsdale Road and two townhouses at the rear of the site.

More specifically, the report:

- Provides an overview of the site's physical context;
- Provides a description of the proposal;
- Provides a summary of the applicable statutory planning framework as set out in the Queenscliffe Planning Scheme; and
- Provides an assessment of the proposal having regard to the relevant provisions of the Queenscliffe Planning Scheme.

A planning permit is required pursuant to the provisions of the Commercial 1 Zone and Design and Development Overlay (Schedule 2).

The project team comprises:

Three C Architects
Project Architects

Contour Consultants
Town Planners





2 Subject Site and Surrounding Area

2.1 Subject Site

As illustrated at Figures 2.1, 2.2, 2.3 and 2.4, the subject land is located on the west side of Point Lonsdale Road, one property to the north of Admans Street in Point Lonsdale.

The site is irregular in shape with a frontage to Point Lonsdale Road of 9.88 metres and a western abuttal to Simpson Street of 8.56 metres. The land comprises an overall site area of 465 m².

The subject site is formally described as Lot 1 LP140111 Volume 8146 Folio 578.

The site currently contains a two storey building fronting Point Lonsdale Road that is currently used for retail purposes at ground level and a dwelling at first floor level. To the rear, the land is currently informally used for car parking and storage.

2.2 Surrounding Area

The subject site is located at the southern end of the Point Lonsdale commercial centre that extends from Kirk Road to the north to Admans Street to the south. The commercial centre is defined by the Commercial 1 Zone and the Design and Development Overlay (Schedule 2).

To the north is an existing single storey shop at No. 61 Point Lonsdale Road. The shop appears to be provided with vehicle access via the rear of properties at No.'s 63-69 Point Lonsdale Road.

To the south, at No. 51 Point Lonsdale Road, is a relatively recent three storey building on the corner of Admans Street that comprises ground level retail premises and dwellings above. This site also accommodates two two-storey townhouses to the rear that front Simpson Street.

To the west, across Simpson Street are a range of one and two storey dwellings.

To the east, across Point Lonsdale Road is the Point Lonsdale Bowling Club within the foreshore reserve.

2



57-59 Point Lonsdale Road, Point Lonsdale

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2 Subject Site and Surrounding Area

Figure 2.1
LOCALITY PLAN

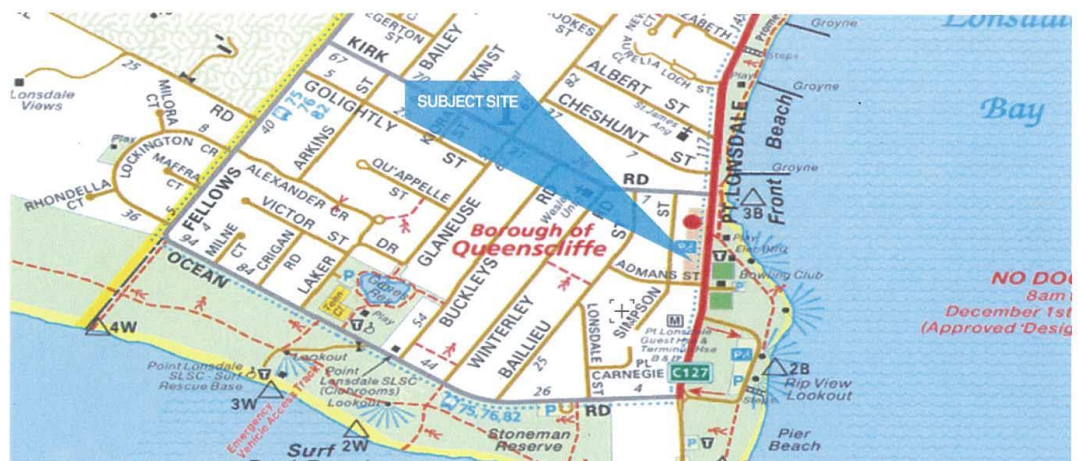


Figure 2.2
CADASTRAL PLAN





57-59 Point Lonsdale Road, Point Lonsdale

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2 Subject Site and Surrounding Area

Figure 2.3
AERIAL PHOTOGRAPH – BROAD CONTEXT



Figure 2.4
AERIAL PHOTOGRAPH





3 Statutory Planning Framework

3.1 Summary of Planning Controls

The subject site is affected by the Queenscliffe Planning Scheme (QPS).

The following provisions within the QPS apply to the subject site:

- Commercial 1 Zone
- Design and Development Overlay (Schedule 2)
- Vegetation Protection Overlay (Schedule 1)

3.2 State Planning Policy Framework

The following elements of the State Planning Policy Framework (SPPF) are considered relevant to the assessment of the proposal:

- Settlement (Clause 11)
 - Regional Settlement Networks (Clause 11.05-1)
 - Coastal Settlement (Clause 11.05-5)
 - Geelong (G21) Regional Growth (Clause 11.07)
- Coastal Areas (Clause 12.02)
- Environmental Risks (Clause 13)
- Built Environment and Heritage (Clause 15)
 - Urban Design (Clause 15.01-1)
- Housing (Clause 16)
 - Residential Development (Clause 16.01)
- Economic Development (Clause 17)
 - Business (Clause 17.01-1)
 - Tourism (Clause 17.03)
 - Maritime Precincts (Clause 17.03-3)

3.3 Local Planning Policy Framework

Municipal Strategic Statement (MSS)

Clause 21 within the Queenscliffe Planning Scheme contains the Municipal Strategic Statement (MSS). The MSS provides the strategic direction for land use and development objectives for the Borough of Queenscliffe, and those policies that are of relevance to the proposal, include:

- Municipal Profile (Clause 21.02)
- Key Influences (Clause 21.03)
- Vision – Strategic Framework (Clause 21.04)
- Objectives Strategies and Implementation (Clause 21.05)

Municipal Profile (Clause 21.02)

Clause 21.02 recognises the township of Point Lonsdale as moving away from its traditional function of providing for the everyday needs of the community (basic convenience retailing) with restaurants, services and speciality shops emerging to meet the tourist led demand.





3 Statutory Planning Framework

Key Influences (Clause 21.03)

Key influences on land use planning are identified within Clause 21.03 as follows:

- Ageing Population
- Substantial temporary population
- Limited employment opportunities
- Limited land availability
- Management of tourism, built and cultural heritage along with natural environment.

Vision (Clause 21.04)

The vision for the Borough is:

The Borough of Queenscliffe will maintain its unique natural environment. Built and cultural heritage values through the careful planning of land use and development for the benefit of the community.

In order to implement this vision, the following strategies found at Clause 21.04 are relevant to the proposal:

- Promote and facilitate sustainable economic development
- Provide opportunities for improved retail and commercial ventures without undue impact on existing amenity;
- Provide attractive residential areas which are capable of accommodating the needs of resident and tourist populations, whilst maintaining residential amenity;
- Require new developments to maintain, enhance, and harmonise with the Borough's significant heritage architecture and coastal village atmosphere;
- Allow for growth in clearly defined locations;
- Resist inappropriate development proposals which compromise acknowledged natural, built and cultural environmental values

Furthermore, the policy identifies the community vision for the township of Point Lonsdale as :

A coastal village with reduced impact on the environment. A place of green vistas, unspoilt coastal views, negotiable walking paths and not impacted by clustered development and traffic congestion.

Within the Point Lonsdale Structure Plan map (Map D2) the subject site is located within the Neighbourhood Activity Centre.



3 Statutory Planning Framework

Objectives – Strategies – Implementation (Clause 21.05)

Clause 21.05-1 relates to 'Settlement', and the following objectives of this clause are relevant to the proposal:

- *Allow for growth in clearly defined locations.*
- *Avoid inappropriate built development that compromises areas of acknowledged natural, built, cultural or environmental sensitivity*

Clause 21.05-2 also refers to 'Urban Character' and seeks to identify and protect the valued urban character elements of the Borough.

Clause 21.05-3 relates to 'Living' and seeks to ensure that the valued residential environment for both residents and temporary residents is maintained. This will be achieved by improving the range of housing options, encouraging increased housing diversity and appropriate density, and ensuring that all new development is of high quality design.

It is noted that medium density housing development is encouraged where identified natural, built heritage, landscape and cultural values are not compromised.

Furthermore, development that retains and makes use of existing buildings of heritage significance or that contributes positively to the distinct character of the area should be supported.

Clause 21.05-4 relates to 'Economic Development'. As it relates to the proposal, this clause seeks to preserve and enhance the character of retail and commercial centres and to encourage retail and commercial employment.

Local Planning Policies

In addition to the MSS, there are a number of local policies contained within the Queenscliffe Planning Scheme which are relevant to the proposed development and these are outlined below:

- Urban Character Policy (Clause 22.04)

Urban Character Policy (Clause 22.04)

Clause 22.04 applies to the entire municipality and to three sub precincts.

Relevant objectives include, inter alia:

- *To recognise and protect the significant cultural heritage and natural coastal atmosphere of the Borough which distinguishes its special character;*
 - *To ensure new development in Point Lonsdale maintains, enhances and harmonises with the distinguishing natural coastal identity of the township;*
-



3 Statutory Planning Framework

- To require new development in Point Lonsdale to have regard to the retention of existing vegetation and the prevailing scale, height, siting, fencing and driveway design, materials and finishes of buildings and works in the natural coastal area;
- To minimise the visual impact of new development in foreshore areas, having regard to building height and topographical features of the land, bulk, site coverage, external materials and finishes and the removal of vegetation; and
- To require all new development to have regard to the siting and design objectives and guidelines of the Borough of Queenscliffe Urban Character Study.

It is policy that:

New development in the Borough will seek to protect, enhance and harmonise with the following distinguishing elements of the Borough's urban character:

- The unique and intact building, landscape heritage, and natural foreshore qualities of the Borough;
- The sense of seclusion and intimacy of Point Lonsdale, created through its low impact (single and two storey), recessive built form and informal, narrow streets in a natural landscape dominant environment;
- The distinctive urban heritage and natural coastal settings of Queenscliff and Point Lonsdale respectively.
- Before deciding on an application for a planning permit for any buildings and works, the responsible authority must consider:
 - Whether the proposal protects, enhances and harmonises with the distinguishing elements of the Borough's urban character and the character of the specific area in which it is located;
 - The objectives and basis of this policy;

Clause 22.04-3 is specific to the foreshore areas within Point Lonsdale.

The following objectives are relevant:

- Significant views of Port Phillip Bay and Swan Bay from buildings, roads and other public spaces in the area;
- The substantial grouping of 1960s to contemporary 2 storey detached dwellings overlooking Swan Bay which are highly visible from many northern parts of the Borough;
- The established built edge to Pt Lonsdale Road comprising 1 and 2 storey post world war buildings facing the Port Philip dune, which creates a strong urban entrance into the township;
- The open relationship of buildings to Swan Bay created by deep front gardens and no or transparent front fences;
- Deep front gardens with established vegetation along Point Lonsdale Road;
- The informality of streetscape materials and finishes and lack of formal footpaths, verges, front fencing and constructed driveways;



3 Statutory Planning Framework

- The prevailing native coastal dune landscape within the Port Philip Bay Area which, in conjunction with the undulating landform, conceals 1 and 2 storey buildings from view along the foreshore; and
- The generally low profile of buildings in the Point Lonsdale commercial area

It is policy that:

Site Layout

- The layout of new development minimises any visual intrusion of buildings or any other elements into the natural foreshore and dune setting.
- The layout of new development is consistent in terms of the location of existing buildings on the site or associated adjacent buildings.
- The layout of new buildings, paved areas, and driveways, car parking, active outdoor living areas and landscape areas have regard to the site's physical, historic and environmental features and the location of buildings on adjacent land.
- The layout of new development provide a high quality of living for occupants of the site and maintains an appropriate standard of amenity for the occupants of adjoining dwellings.
- The layout of new development gives regard to the need for future extensions or additions on the site at ground level (i.e. total site planning).
- On sites where vegetative screens are insubstantial, the layout of new development should give regard to providing the opportunity for landscaped areas in appropriate locations to be planted with tea-tree and moonah to screen buildings from view, extend the roadside vegetation corridor, and reinstate the natural environment.
- The layout of new development minimises the removal of any existing native coastal vegetation on the site and adjacent road reserve.
- New development is sited on the flattest portion of the land wherever possible, so as to minimise the need for earthworks and significant disturbance of the land.

New development should meet the following design standards:

- Where possible, provide north facing indoor and outdoor living areas to maximise solar access;
- A frontage to the street and sense of address;
- Siting which minimises views to the building from the street and other public places and provides opportunities for native coastal planting in front of the building and between adjacent buildings in the street;
- Building layout which limits the extent of earthworks required to a maximum 1 metre;
- Maximum site coverage of 40% (except in the Point Lonsdale Shopping Village) and minimal hard surface areas; and



3 Statutory Planning Framework

-
- *Avoid siting buildings on or near ridgelines or more prominent or visible parts of the site (especially as viewed from along the Port Phillip Bay foreshore).*

Building Design

- *The design of new development blends in with the prevailing coastal character of the area and expresses an appropriate coastal design that interprets and reflects the architectural style, massing, scale, proportion and materials and finishes of established resort/holiday style buildings in the area.*
- *The design of new development gives regard to any features on the land, any sensitivities associated with coastal environments, and any view corridors from the streets, public places, surrounding properties, foreshore areas and the bay.*
- *New building design makes efficient use of the site, especially natural ventilation, daylight and solar energy without compromising the natural coastal quality of the area.*
- *New development gives regard to the style, form and scale of any primary existing building on the land, and minimises its visual impact within the foreshore setting.*

New development should meet the following design standards:

- *Staggered and varied roof lines (including pitches or curves) and building line to reduce the visual bulk of the building;*
- *Innovative coastal design themes, including the use of lightweight, glazed and metallic materials, open decks and balconies, curved roofs and refreshing coastal colours;*
- *Spilt level design to better integrate the building into a sloping site and minimise the need for earthworks;*
- *Robust coastal building materials, including the sensitive use of horizontal and vertical weatherboard wall cladding, light tone rendered brickwork, Colourbond roofs, timber and stainless steel verandahs, doors and windows and painted finishes;*
- *Innovative building design techniques and materials that reduce energy use and are responsive to coastal conditions;*
- *Avoid direct overlooking into neighbouring properties and minimise the loss of significant views across the site from neighbouring properties; and*
- *In the Port Phillip Bay and Point Lonsdale Road Areas, the use of colour finishes which are non-reflective and of natural tones which blend in with the vegetated coastal setting*

Building Height and Setbacks

- *The height and setback of new development retain the existing prevailing rhythm, height and setback along both the street and the foreshore.*
 - *The height and setback of new development ensure that no individual building or addition dominates the streetscape, dune or coastal landscape.*
 - *The height of new development does not protrude above the ridge line of the dunes, or in particularly sensitive locations, project above the coastal tree canopy to form a silhouette*
-



3 Statutory Planning Framework

against the sky when viewed from the foreshore or any significant public viewing point.

- The height and setback of new development ensures adequate daylight to all habitable rooms and private open spaces on the land.
- The siting of new development, where appropriate, provides the opportunity for the foundation of established gardens in front setbacks to contribute to the coastal and streetscape setting.
- Building height which is not significantly higher than the height of adjoining buildings or other buildings in the street;
- A front setback which matches the setback of adjoining buildings and where these setbacks are different, a front setback which is between the setbacks of adjoining buildings or equal to the average setback of buildings in the street, whichever is the lesser of the two;
- A partial second storey if buildings are openly visible from the street;
- Avoids building across the width of the site, from boundary to boundary.

Fencing, Driveways and Landscaping

- Fencing, particularly to the site's front or coastal boundary in the Swan Bay foreshore area, relates strongly to the open and transparent character of this coastal setting.
- Fencing, particularly along the front of the site, takes into account the open configuration and visually transparent treatment along the foreshore.
- New driveways and paths provide adequate access and minimise surface drainage runoff in coastal or dune environments.
- New driveways and parking areas are not visually dominant at the street or from public spaces or are screened by appropriate frontage landscaping.
- The design of outbuildings gives regard to the style and form, roof shape and wall and roof materials and finishes of existing buildings on the site and are visually screened from street or foreshore view.
- New landscaping is provided which complements existing plantings on site with appropriate new native or indigenous planting to assist in blending-in of new development into the coastal setting.
- Where appropriate, landscaped setbacks to side boundaries are provided to minimise the potential for overlooking, whilst providing a visual landscape link to rear garden and private open space areas.

New development should meet the following design standards:

- Curved, narrow and unsealed driveways, which have minimal impact on existing vegetation and use gravel and other permeable pavement materials;
- Utilisation of the natural slope of the land or mounding to screen buildings from view in the Port Phillip Bay foreshore area and from roads;



3 Statutory Planning Framework

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- *Adoption of visually open boundary fencing to the site's foreshore boundary (where applicable);*
 - *Use of coastal heath and moonah in landscaping of private gardens;*
 - *Discouragement of front fences in the Point Lonsdale foreshore area;*
 - *Timber post and wire fences to a maximum height of 1.2 metres or no fences along the street frontage in the Swan Bay foreshore area; and*
 - *Retention of existing native coastal vegetation on the land wherever possible.*

Within the Urban Character Policy Map 3, the subject site is located within Precinct 3 (Point Lonsdale Road).



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3.4 Zoning

The subject site is included within the Commercial 1 Zone (C1Z).

The purpose of the Commercial 1 Zone is:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To create vibrant mixed use commercial centres for retail, office, business, entertainment and community uses.
- To provide for residential uses at densities complementary to the role and scale of the commercial centre

Pursuant to the provisions of the CZ1, use of the land for 'Dwelling' and 'Shop' are Section 1 permit not required uses, subject to any dwelling frontage at ground floor level is less than 2 metres.

Pursuant to the provisions of the CZ1, a permit is required to construct a building or construct or carry out works.

Figure 3.1
ZONING MAP





3 Statutory Planning Framework

3.5 Overlays

The subject site is affected by the following Overlays:

- Design and Development Overlay (Schedule 2)
- Vegetation Protection Overlay

Design and Development Overlay (Schedule 2)

The purpose of the Design and Development Overlay is:

- *To implement the State Planning Policy Framework and Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.*
- *To identify areas which are affected by specific requirements relating to the design and built form of new development.*

Schedule 2 to the Design and Development Overlay - Point Lonsdale Shopping Village

The design objectives include:

- *To ensure new development within the shopping centre is sympathetic to the distinguishing elements of the urban character of the Point Lonsdale Natural Coastal Area and to the Foreshore Area which abut the centre.*
- *To encourage urban design improvements so that the physical environment offers variety, interests, safety and convenience to people using the centre.*
- *To ensure building height, bulk and setbacks are sympathetic with adjoining sites and distinguish the commercial area of Point Lonsdale from adjacent residential areas.*
- *To consolidate and distinguish the commercial functions of the Point Lonsdale shopping centre from adjacent residential land.*

Pursuant to Schedule 2 to the DDO, a permit is required to construct a building or carry out works.

Furthermore, no building can exceed a height of:

- Three storeys; and
- 9.5 metres above natural ground level.

Other requirements include:

- *Service delivery and carparking facilities should be provided from the rear.*
- *Building design should have regard to its appearance from the foreshore and nearby public spaces.*
- *Consideration should be given to the use of balconies, verandahs and architectural features to achieve consistency with the existing built form.*



3 Statutory Planning Framework

Before deciding on an application for a permit the Responsible Authority must consider:

- The design objectives of this schedule.
- Whether the proposed development satisfies the local planning policy contained in Clause 22.04 of the Planning Scheme.
- The effect of the development on the foreshore and surrounding public areas.
- The Building Siting and Design Guidelines contained in the Borough of Queenscliffe Urban Character Study, where relevant.
- Compliance with any adopted streetscape policy or plan.

Vegetation Protection Overlay Schedule 1

The purpose of the Vegetation Protection Overlay is:

- To implement the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.
- To protect areas of significant vegetation.
- To ensure that development minimises loss of vegetation.
- To preserve existing trees and other vegetation.
- To recognise vegetation protection areas as locations of special significance, natural beauty, interest and importance.
- To maintain and enhance habitat and habitat corridors for indigenous fauna.
- To encourage the regeneration of native vegetation.

Pursuant to Clause 42.02-2, a permit is required to remove, destroy or lop any vegetation specified in a schedule to the overlay. Schedule 1 applies to Remnant and Vegetation Protection Areas.

Figure 3.2
DESIGN AND DEVELOPMENT OVERLAY SCHEDULE 2





3 Statutory Planning Framework

3.6 Particular and General Provisions

The following particular and general provisions are relevant to the consideration of the application:

- Clause 52.06 –Car Parking
- Clause 52.07 –Loading and Unloading of Vehicles
- Clause 65 –Decision Guidelines



4 Proposal

The application proposes the construction of two buildings.

At the Point Lonsdale Road frontage, the building comprises:

Ground Level

- A retail tenancy with an overall floor area of 66 m²
- Lift and stair access to the two apartments.
- Pedestrian entry to the dwellings from Point Lonsdale Road.

First Floor Level

- One apartment with four bedrooms, an open plan kitchen/living area and a 20m² deck facing Point Lonsdale Road.

Second Floor Level

- One apartment with four bedrooms, an open plan kitchen/meals/living/tv/play area and a 20m² deck facing Point Lonsdale Road.

Roof Level

- A roof deck is provided for the second floor level apartment. The deck is setback 7.755m from Point Lonsdale Road, 3.81m from the northern boundary and 3.015m from the southern boundary.

At the rear are two 3 storey townhouses that include entries, bedrooms, bathroom and laundries on ground level, open plan living/kitchen/meals areas and balconies on the first floor and a master bedroom and second balcony on the upper level.

The proposed buildings comprise a maximum height of 9.5 metres.

A total of 7 car spaces are proposed, six of which are accessed via the common driveway along the southern boundary. The seventh space is located within the front setback, behind the proposed front fencing..

Further detail in relation to the architectural response, design and materials and finished is provided in the application material prepared by Three C Architects.

4



5 Planning Assessment

5.1 Preamble

Having regard to the provisions of the Queenscliffe Planning Scheme, an assessment of the proposed development requires an analysis of the following key considerations:

- The strategic policy support for the proposed development having regard for the applicable State and Local Policy Frameworks.
- The appropriateness of the design and built form having regard to the site's physical context including pattern and character of surrounding development;
- The contribution of the proposed development to the public realm; and
- Assessment of the potential external amenity impacts.

An assessment of each of these key consideration is provided in the following sections of this report.

5.2 Strategic Policy Considerations

There is considerable policy support for the proposal at both State and local levels.

In terms of State Planning Policy, the G21 Regional Growth Plan identifies the site as being located within an existing settlement where key objectives include building on tourism opportunities and natural assets.

At the local level, the QPS recognises the shift away from the traditional function of basic convenience retailing to restaurants and specialty shops that meet tourist led demand. The proposal responds to these policy aspirations and the applicable economic development objectives by proposing a retail premises at ground level.

In terms of housing, the QPS seeks to maintain a stable resident population while also allowing for growth in appropriate locations, noting that growth should occur within the settlement boundary and within the commercial centres as proposed.

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5 Planning Assessment

5.3 Built Form Considerations

The relevant provision in determining the suitability of the proposed built form are set out in:

- Clause 22.04 - Urban Character Policy
- Clause 43.02 - Design and Development Overlay (Schedule 2)

An assessment of these provisions is set out below.

Urban Character Policy (Clause 22.04)

Objective	Response
<i>To recognise and protect the significant cultural heritage and natural coastal atmosphere of the Borough which distinguishes its special character;</i>	Whilst the site is located opposite the foreshore reserve, it is within the Point Lonsdale commercial centre. The commercial centre is identified by the application of DDO2, and it recognises the ability for this location to accommodate buildings of up to three storeys (as proposed) that are distinguished from the residential hinterland to the west.
<i>To ensure new development in Point Lonsdale maintains, enhances and harmonises with the distinguishing natural coastal identity of the township;</i>	The architectural response and proposed materials and finishes achieve a balance between the site's location within a commercial centre but also opposite the foreshore reserve.
<i>To require new development in Point Lonsdale to have regard to the retention of existing vegetation and the prevailing scale, height, siting, fencing and driveway design, materials and finishes of buildings and works in the natural coastal area;</i>	<p>There is currently no existing vegetation on the site, and the planting area towards the rear of the site will enable the planting of a canopy tree.</p> <p>With regard to the scale, height, siting, fencing, driveway design and the proposed materials, all are a direct response to the coastal environs and the preferred character for the commercial centre.</p>
<i>To minimise the visual impact of new development in foreshore areas, having regard to building height and topographical features of the land, bulk, site coverage, external materials and finishes and the removal of vegetation; and</i>	As above.
<i>To require all new development to have regard to the siting and design objectives and guidelines of the Borough of Queenscliffe Urban Character Study.</i>	The proposal has regard to the siting and design objectives and guidelines of the Borough of Queenscliffe Urban Character Study in so far as they relate to the commercial area.



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Policy	Response
<i>New development in the Borough will seek to protect, enhance and harmonise with the following distinguishing elements of the Borough's urban character:</i>	
→ <i>The unique and intact building, landscape heritage, and natural foreshore qualities of the Borough;</i>	The proposal is consistent with the built form outcomes envisaged for the commercial centre in terms of height, mass, form and materials.
→ <i>The sense of seclusion and intimacy of Point Lonsdale, created through its low impact (single and two storey), recessive built form and informal, narrow streets in a natural landscape dominant environment;</i>	While it is recognised that the land is in the Commercial 1 Zone, the inclusion of townhouses provides a transition from the commercial functions of this section of Point Lonsdale Road to the residential precinct to the west.
→ <i>The distinctive urban heritage and natural coastal settings of Queenscliff and Point Lonsdale respectively.</i>	The proposal has been designed to be consistent with the applicable land use and built form provisions of the Queenscliffe Planning Scheme and it will comfortably integrate with the urban heritage and natural coastal setting.
→ <i>Before deciding on an application for a planning permit for any buildings and works, the responsible authority must consider:</i>	
→ <i>Whether the proposal protects, enhances and harmonises with the distinguishing elements of the Borough's urban character and the character of the specific area in which it is located;</i>	As already noted, the proposal is consistent with the land use and built form outcomes envisaged for the commercial centre, and it achieves a balance between its commercial zoning and the 'coastal' character.
→ <i>The objectives and basis of this policy;</i>	Refer to assessment above.
→ <i>The Borough of Queenscliffe Urban Character Study and its Building Siting and Design Guidelines; and</i>	Refer to assessment below.
→ <i>Whether the proposal satisfies the relevant policy below and if discretion from the policy is sought, whether the proposal accords with the objectives and policies as a whole.</i>	Refer to overall assessment.



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Clause 22.04-3 Foreshore Areas	
Objective	Response
→ Significant views of Port Phillip Bay and Swan Bay from buildings, roads and other public spaces in the area;	The proposal will not obstruct views of Port Phillip Bay when viewed from Point Lonsdale Road or the foreshore reserve.
→ The substantial grouping of 1960s to contemporary 2 storey detached dwellings overlooking Swan Bay which are highly visible from many northern parts of the Borough;	N/A
→ The established built edge to Pt Lonsdale Road comprising 1 and 2 storey post world war buildings facing the Port Phillip dune, which creates a strong urban entrance into the township;	Not applicable to the commercial centre.
→ The open relationship of buildings to Swan Bay created by deep front gardens and no or transparent front fences;	N/A
→ Deep front gardens with established vegetation along Point Lonsdale Road;	Not applicable to the commercial centre.
→ The informality of streetscape materials and finishes and lack of formal footpaths, verges, front fencing and constructed driveways;	Not applicable to the commercial centre.
→ The prevailing native coastal dune landscape within the Port Phillip Bay Area which, in conjunction with the undulating landform, conceals 1 and 2 storey buildings from view along the foreshore; and	The proposed building is concealed from view along the foreshore by the existing bowling club.
→ The generally low profile of buildings in the Point Lonsdale commercial area	The proposal has been designed to comply with the applicable DDO provisions, and it is consistent with the height of the adjoining development to the south and the preferred character of the commercial centre.



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Policy	Response
Site Layout	
→ <i>The layout of new development minimises any visual intrusion of buildings or any other elements into the natural foreshore and dune setting.</i>	As already noted, the proposed buildings are located within the existing commercial centre and is concealed from view along the foreshore by the existing bowling club.
→ <i>The layout of new development is consistent in terms of the location of existing buildings on the site or associated adjacent buildings.</i>	The proposed site planning strategy responds to the character and pattern of development in the commercial centre at its Point Lonsdale Road frontage and to the residential precinct to the rear.
→ <i>The layout of new buildings, paved areas, and driveways, car parking, active outdoor living areas and landscape areas have regard to the site's physical, historic and environmental features and the location of buildings on adjacent land.</i>	The proposed site planning strategy and built form has been arranged so that there are two distinct building typologies within a functional program of access / movement, and the outcome will be a high quality mixed use environment.
→ <i>The layout of new development provide a high quality of living for occupants of the site and maintains an appropriate standard of amenity for the occupants of adjoining dwellings.</i>	<p>The proposed dwelling layouts provide a high level of internal amenity for future occupants.</p> <p>All dwellings are provided with good access to daylight, opportunities for cross ventilation, decks or balconies accessed directly from living areas and no opportunities for overlooking or direct views between dwellings on the site.</p>
→ <i>The layout of new development gives regard to the need for future extensions or additions on the site at ground level (i.e. total site planning).</i>	Future additions or extensions are not anticipated.
→ <i>On sites where vegetative screens are insubstantial, the layout of new development should give regard to providing the opportunity for landscaped areas in appropriate locations to be planted with tea-tree and moonah to screen buildings from view, extend the roadside vegetation corridor, and reinstate the natural environment.</i>	The proposal provides opportunities for landscaping within the car parking area to the rear, whilst landscaping to Point Lonsdale Road is not characteristic of the commercial context.
→ <i>The layout of new development minimises the removal of any existing native coastal vegetation on the site and adjacent road reserve.</i>	No existing vegetation within the adjacent road reserve will be removed.
→ <i>New development is sited on the flattest portion of the land wherever possible, so as to minimise the need for earthworks and significant disturbance of the land.</i>	The proposal does not anticipate requiring significant earthworks.



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New development should meet the following design standards:	
→ Where possible, provide north facing indoor and outdoor living areas to maximise solar access;	Given the east-west orientation of the land, the proposal provides opportunities for north facing living areas.
→ A frontage to the street and sense of address;	From Point Lonsdale Road, a clearly identifiable pedestrian entry is provided to the retail premises, with a separate pedestrian entry provided to the south for access to the dwellings. From Simpson Street, the proposed driveway provides shared vehicle and pedestrian access to the site, with each townhouse being provided with its own entry. All dwelling entries are secured via a pedestrian gate from Point Lonsdale Road and timber tilt up panel door or timber picket gate from Simpson Street.
→ Siting which minimises views to the building from the street and other public places and provides opportunities for native coastal planting in front of the building and between adjacent buildings in the street;	<p>The proposal is visible from Point Lonsdale Road, consistent with the scale of buildings expected within the commercial centre.</p> <p>The proposal will also be visible from Simpson Street, which is considered appropriate given its location within the commercial centre.</p>
→ Building layout which limits the extent of earthworks required to a maximum 1 metre;	The proposed building has been sited and designed to minimise the extent of earthworks required.
→ Maximum site coverage of 40% (except in the Point Lonsdale Shopping Village) and minimal hard surface areas; and	Proposed site coverage is 69% which is considered appropriate having regard to the site's commercial zoning.
→ Avoid siting buildings on or near ridgelines or more prominent or visible parts of the site (especially as viewed from along the Port Phillip Bay foreshore).	N/A
Building Design	
The design of new development blends in with the prevailing coastal character of the area and expresses an appropriate coastal design that interprets and reflects the architectural style, massing, scale, proportion and materials and finishes of established resort/holiday style buildings in the area.	The proposed design aesthetic is one that is suited to the coastal location and the proposed materials include timber decking, panelling and cladding, timber battens, corrugated roof sheeting, metal finishes, precast concrete and glazed balustrades.



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→ The design of new development gives regard to any features on the land, any sensitivities associated with coastal environments, and any view corridors from the streets, public places, surrounding properties, foreshore areas and the bay.	The design response has had regard to the features of the land which include the slope, road frontages and existing buildings on adjoining properties, and there are no view corridors affecting the land that are identified as significant.
→ New building design makes efficient use of the site, especially natural ventilation, daylight and solar energy without compromising the natural coastal quality of the area.	<p>The proposed buildings make efficient use of the site by incorporating dwellings into two three-storey buildings. This reduces the footprint of the buildings and provides an opportunity to provide a break between the two forms.</p> <p>Dwellings are also provided with opportunities for cross ventilation, access to daylight and solar energy by virtue of suitably oriented and located operable and fixed windows and doors.</p>
→ New development gives regard to the style, form and scale of any primary existing building on the land, and minimises its visual impact within the foreshore setting.	N/A
New development should meet the following design standards:	
→ Staggered and varied roof lines (including pitches or curves) and building line to reduce the visual bulk of the building;	The proposed building provides a sloping roof to Point Lonsdale Road that follows the fall of the land and a contemporary interpretation of a gabled roof form for the townhouses at the rear.
→ Innovative coastal design themes, including the use of lightweight, glazed and metallic materials, open decks and balconies, curved roofs and refreshing coastal colours;	Significant glazed openings and the use of timber and other cladding materials will assist in articulating the coastal design theme for the buildings.
→ Spilt level design to better integrate the building into a sloping site and minimise the need for earthworks;	The proposed buildings are sited to minimise the need for significant earthworks.
→ Robust coastal building materials, including the sensitive use of horizontal and vertical weatherboard wall cladding, light tone rendered brickwork, Colourbond roofs, timber and stainless steel verandahs, doors and windows and painted finishes;	The proposed palette of materials have been deliberately chosen to respond to the commercial context.
→ Innovative building design techniques and materials that reduce energy use and are responsive to coastal conditions;	The proposed dwellings are designed to maximise passive daylight and ventilation opportunities.



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→ Avoid direct overlooking into neighbouring properties and minimise the loss of significant views across the site from neighbouring properties; and	Opportunities for overlooking of existing dwellings on adjoining properties are minimised via the use of timber batten screening to a height of 1.7m. In addition, screening is provided to the west facing balconies associated with the rear townhouse to minimise any potential for longer range views across Simpson Street.
→ In the Port Phillip Bay and Point Lonsdale Road Areas, the use of colour finishes which are non-reflective and of natural tones which blend in with the vegetated coastal setting	All proposed materials are non-reflective and will be in neutral tones.
Building Height and Setbacks	
→ The height and setback of new development retain the existing prevailing rhythm, height and setback along both the street and the foreshore.	<p>The height of the proposed buildings is consistent with the existing development to the south as well as properties further to the north that are located within the commercial centre.</p> <p>A zero setback to Point Lonsdale Road is provided to ensure a continuous street wall is provided within the commercial centre, and upper levels are set back behind the parapet to ensure these levels are viewed as a recessive element.</p> <p>To the rear, the proposed townhouses are setback further than the existing townhouses to the south.</p>
→ The height and setback of new development ensure that no individual building or addition dominates the streetscape, dune or coastal landscape.	As above.
→ The height of new development does not protrude above the ridge line of the dunes, or in particularly sensitive locations, project above the coastal tree canopy to form a silhouette against the sky when viewed from the foreshore or any significant public viewing point.	N/A
→ The height and setback of new development ensures adequate daylight to all habitable rooms and private open spaces on the land.	Refer to earlier comments.
→ The siting of new development, where appropriate, provides the opportunity for the foundation of established gardens in front setbacks to contribute to the coastal and streetscape setting.	A front setback that includes planting is not appropriate along Point Lonsdale Road.
→ Building height which is not significantly higher than the height of adjoining buildings or other buildings in the street;	Refer to earlier comments.



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→ A front setback which matches the setback of adjoining buildings and where these setbacks are different, a front setback which is between the setbacks of adjoining buildings or equal to the average setback of buildings in the street, whichever is the lesser of the two;	Refer to earlier comments.
→ A partial second storey if buildings are openly visible from the street;	Not applicable to commercial centre.
→ Avoids building across the width of the site, from boundary to boundary.	Not applicable to commercial centre.
Fencing, Driveways and Landscaping	
→ Fencing, particularly to the site's front or coastal boundary in the Swan Bay foreshore area, relates strongly to the open and transparent character of this coastal setting.	A sliding gate is proposed along Simpson Street to provide security to car parking space No. 7. A timber picket gate is set back in line with the rear townhouse to provide security to the remaining car parking spaces. No other fencing is proposed.
→ Fencing, particularly along the front of the site, takes into account the open configuration and visually transparent treatment along the foreshore.	No fencing is proposed to Point Lonsdale Road. The proposed timber picket gate will be visually permeable while providing for the security of the car parking area.
→ New driveways and paths provide adequate access and minimise surface drainage runoff in coastal or dune environments.	The proposed driveway and car parking areas will be appropriately drained.
→ New driveways and parking areas are not visually dominant at the street or from public spaces or are screened by appropriate frontage landscaping.	Vehicle access, driveways and car parking are located to the rear of the proposed buildings, accessed from Simpson Street.
→ The design of outbuildings gives regard to the style and form, roof shape and wall and roof materials and finishes of existing buildings on the site and are visually screened from street or foreshore view.	N/A
→ New landscaping is provided which complements existing plantings on site with appropriate new native or indigenous planting to assist in blending-in of new development into the coastal setting.	Opportunities for new planting are provided within the rear car parking area and within the setback to Simpson Street.
→ Where appropriate, landscaped setbacks to side boundaries are provided to minimise the potential for overlooking, whilst providing a visual landscape link to rear garden and private open space areas.	Landscaped setbacks are not required within the commercial centre.
New development should meet the following design standards:	



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→ Curved, narrow and unsealed driveways, which have minimal impact on existing vegetation and use gravel and other permeable pavement materials;	A narrow, unsealed driveway is not appropriate in this instance.
→ Utilisation of the natural slope of the land or mounding to screen buildings from view in the Port Phillip Bay foreshore area and from roads;	N/A
→ Adoption of visually open boundary fencing to the site's foreshore boundary (where applicable);	N/A
→ Use of coastal heath and moonah in landscaping of private gardens;	The use of coastal heath and moonah can be considered as part of any detailed landscape plan.
→ Discouragement of front fences in the Point Lonsdale foreshore area;	N/A
→ Timber post and wire fences to a maximum height of 1.2 metres or no fences along the street frontage in the Swan Bay foreshore area; and	N/A
→ Retention of existing native coastal vegetation on the land wherever possible.	N/A

Design and Development Overlay (Schedule 2) (Clause 43.02)

It is noted that the proposed building meets the building height requirements by providing buildings that do not exceed three storeys and 9.5 metres above natural ground level.

The following provides a response to the design objectives and policies of Clause 43.02:

Design Objective	Response
<i>To ensure new development within the shopping centre is sympathetic to the distinguishing elements of the urban character of the Point Lonsdale Natural Coastal Area and to the Foreshore Area which abut the centre.</i>	As set out in the preceding response to Clause 22.04, the proposal represents a considered response to the coastal character, but at the same time it is a proposal that is influenced by its commercial zoning and commercial centre context.
<i>To encourage urban design improvements so that the physical environment offers variety, interests, safety and convenience to people using the centre.</i>	The urban design improvements or values are: <ul style="list-style-type: none">• An engaging ground level interface to the public realm;• A scale of development that is consistent with the applicable building height provisions;• A highly articulated form with a material palette that is appropriately 'coastal';• Two separate building forms;• A functional site planning strategy;• A more domestic design approach to the rear.



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<i>To ensure building height, bulk and setbacks are sympathetic with adjoining sites and distinguish the commercial area of Point Lonsdale from adjacent residential areas</i>	The proposed buildings are consistent with the scale of buildings within the commercial area. More specifically the proposed buildings reflect the prevailing building layouts within the commercial area that include zero setbacks at ground level, upper level setbacks with balconies and articulation, no side setbacks with buildings generally built wall to wall and rear setbacks that have been driven by amenity considerations.
<i>To consolidate and distinguish the commercial functions of the Point Lonsdale shopping centre from adjacent residential land.</i>	The proposal is consistent with the Point Lonsdale Structure Plan which seeks to provide new buildings with active ground floor frontages to the boundary along Point Lonsdale Road.

5.4 Traffic Engineering Considerations

Car parking is provided for each dwelling in accordance with the rates set out at Clause 52.06.

One car parking space is provided to each two bedroom townhouse and two car parking spaces are provided to the larger four bedroom apartments.

While it is noted that two car parking spaces are required to be provided for the retail premises, one car parking space is provided for use by staff of the retail premises and customer car parking will continue to be provided within the local street network. A waiver of one space is considered appropriate.

While it is noted a loading bay is required to be provided for the retail premises, it is considered that the loading requirements associated with the premises are likely to be low, and any deliveries are likely to be via small vehicles that can utilise the existing on-street loading bay to the north or the on street parking that is available within close proximity of the site.

On that basis a waiver of the loading bay requirement is considered appropriate.



6 Conclusion

In summary:

- The proposal will assist in the implementation of the objectives of the G21 Regional Growth Plan and the State Planning Policies associated provision of well-designed commercial and residential development in coastal townships.
- The proposal for a mixed use building, with active street level frontages, is consistent with the policy provisions of the Local Planning Policy Framework and the objectives of the Commercial 1 Zone.
- The proposal will provide a positive addition to the Point Lonsdale Road and Simpson Street streetscapes.
- The proposal adopts a scale and height that is consistent with surrounding development, as directed by local planning policy and the Design and Development Overlay (Schedule 2).
- The ground floor of the development will create an active frontage and visual interest and will improve this section of Point Lonsdale Road.
- The proposed built form is an appropriate architectural response having regard to the applicable design criteria contained within the Planning Scheme.
- The proposal will not unreasonably affect the amenity of surrounding properties and has provided appropriate spacing to surrounding properties to maintain access to daylight and protect privacy.
- The proposal will provide a good level of amenity for the future occupants of the dwellings.

Overall, the building is a highly resolved, high quality building that is entirely appropriate in its context and will make a positive contribution to the provision of retail and dwellings within the Point Lonsdale commercial centre, consistent with planning policy.

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Planning Permit Application

57-59 point Lonsdale road, Point Lonsdale
12 March 2015

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Introduction

The title information is Lot 24 on Title Plan LP140111.
The relevant local authority is the Borough of Queenscliffe.

The site area is 465m².

The applicant is seeking approval to demolish the existing dwelling and construct 2, 2 storey townhouses.

10 State Planning Policy

20 Local Planning Policy

22.04 Objectives

To recognise and protect the significant cultural heritage and natural coastal atmosphere of the Borough which distinguishes its special character.

To ensure new development in Queenscliffe maintains, enhances and is harmonious with the distinguishing cultural heritage identity of the township.

To ensure new development in Point Lonsdale maintains, enhances and harmonises with the distinguishing natural coastal identity of the township.

To ensure new development does not reduce the integrity of significant areas of intact native or remnant indigenous vegetation within Point Lonsdale and along The Narrows.

To provide for the enhancement of private gardens and public roads in Point Lonsdale, using native vegetation of local provenance where appropriate, to contribute to the natural coastal character of the township, particularly along road reserves and in front gardens of private property.

To require new development in Queenscliffe to have regard to the prevailing scale, style, height, siting, fencing, building materials and finishes of buildings in the core heritage centre of the town.

To require new development in Point Lonsdale to have regard to the retention of existing vegetation and the prevailing scale, height, siting, fencing and driveway design, materials and finishes of buildings and works in the natural coastal area.

To minimise the visual impact of new development in foreshore areas, having regard to building height and topographical features of the land, bulk, site coverage, external materials and finishes and the removal of vegetation; and

To require all new development to have regard to the siting and design objectives and guidelines of the Borough of Queenscliffe Urban Character Study.

Policy

New development in the Borough will seek to protect, enhance and harmonise with the following distinguishing elements of the Borough's urban character:

The significant view lines to and from the sea, coastal dune environments and the Queenscliffe townscape style:

The unique and intact building, landscape heritage, and natural foreshore qualities of the Borough.

The sense of seclusion and intimacy of Point Lonsdale, created through its low impact (single and two storey), recessive built form and informal, narrow streets in a natural landscape dominant environment.

The prevailing Victorian and Edwardian built form and scale of Queenscliffe and Lonsdale respectively.

Before deciding on an application for a planning permit for any buildings and works, the responsible authority must consider:

Whether the proposed projects, enhances and harmonises with the distinguishing elements of the Borough's urban character and the character of the specific area in which it is located;

The objectives and basis of this policy;

The Borough of Queenscliffe Urban Character Study and its Building Siting and Design Guidelines; and

Whether the proposal satisfies the relevant policy below and if discretion from the policy

is sought, whether the proposal accords with the objectives and policies as a whole.

22.04.3 Objectives

To protect the following distinguishing elements of the character of the Foreshore Areas: Significant views of Port Phillip Bay and Swan Bay from buildings, roads and other public spaces in the area.

The substantial grouping of 1960s to contemporary 2 storey detached dwellings overlooking Swan Bay which are highly visible from many northern parts of the Borough;

The established built edge to Point Lonsdale Road comprising 1 and 2 storey post world war buildings facing the Port Phillip dune, which creates a strong urban entrance into the township;

The open relationship of buildings to Swan Bay created by deep front gardens and no or transparent front fences;

Deep front gardens with established vegetation along Point Lonsdale Road;

The informality of landscape materials and finishes and lack of formal footpaths, verges, front fencing and constructed driveways;

The prevailing native coastal dune landscape within the Port Phillip Bay Area which, in conjunction with the undulating landform, conceals 1 and 2 storey buildings from view along the foreshore; and

The generally low profile of buildings in the Point Lonsdale commercial area.

22.04.3 Policy

It is policy to:

Site Layout

The layout of new development minimises any visual intrusion of buildings or any other elements into the natural foreshore and dune setting.

The layout of new development is consistent in terms of the location of existing buildings on the site or associated adjacent buildings.

The layout of new buildings, paved areas, and driveways, car parking, active outdoor living areas and landscape areas have regard to the site's physical, historic and environmental features and the location of buildings on adjacent land.

The layout of new development provides a high quality of living for occupants of the site and maintains an appropriate standard of amenity for the occupants of adjoining dwellings.

The layout of new development gives regard to the need for future extensions or additions on the site at ground level (i.e. total site planning).

On sites where vegetative screens are insubstantial, the layout of new development should give regard to providing the opportunity for landscaped areas in appropriate locations to be planted with tree and shrub to screen buildings from view, extend the roadside vegetation corridor, and restate the natural environment.

The layout of new development minimises the removal of any existing native coastal vegetation on the site and adjacent road reserve.

New development is sited on the least portion of the land wherever possible, so as to minimise the need for earthworks and significant disturbance of the land.

New development should meet the following design standards:

Where possible, provide north facing indoor and outdoor living areas to maximise solar access.

A fence to the street and sense of address;

Siting which minimises views to the building from the street and other public places and provides opportunities for native coastal planting in front of the building and between adjacent buildings in the street;

Building layout which limits the extent of earthworks required to a maximum 1 metre; Minimum site coverage of 40% (except in the Point Lonsdale Shopping Village) and minimal hard surface areas; and

Avoid siting buildings on or near ridgelines or more prominent or visible parts of the site (especially as viewed from along the Port Phillip Bay foreshore).



	configuration and visually transparent treatment along the foreshore.	
	New driveways and paths provide adequate access and minimise surface drainage run off in coastal or dune environments	The proposed car park will not be highly visible from either Point Lonsdale Road or Simpson St.
	And parking areas are not visually dominant at the street or from public spaces or are screened by appropriate landscape landscaping	
	The design of outbuildings gives regard to the style and form, roof shape and wall and roof materials and finishes of existing buildings on the site and are visually screened from street or foreshore view	Proposed garden bed with canopy trees
	New landscaping is provided which complements existing plantings on site with appropriate new native or indigenous planting to assist in blending of new development into the coastal setting	
	Where appropriate, landscaped setbacks to site boundaries are provided to minimise the potential for overlooking, whilst providing a visual landscape link to rear garden and private open space areas.	
	New development should meet the following design standards:	
	Curved, narrow and unsealed driveways, which have minimal impact on existing vegetation and use gravel and other permeable pavement materials	
	Utilisation of the natural slope of the land or mounding to screen buildings from view in the Port Phillip Bay foreshore area from roads	
	Adoption of visually open boundary fencing to the sites foreshore boundary (where applicable)	
	Use of coastal health and moonah in landscaping of private gardens	
	Discouragement of front fences in the Port Lonsdale foreshore area	
	Timber post and wire fences to a maximum height of 1.2m or no fences along the street frontage in the Swan Bay foreshore area	
	Retention of existing native coastal vegetation on the land wherever possible	



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Building Design	<p>The design of new development blends in with the prevailing coastal character of the area and expresses an appropriate coastal design that interprets and reflects the architectural style, massing, scale, proportion and materials and finishes of established residential style buildings in the area.</p> <p>The design of new development gives regard to any features on the land, any setbacks, public places, surrounding properties, foreshore areas and the bay.</p> <p>New building design makes efficient use of the site, especially natural ventilation, daylight and solar energy without compromising the natural coastal quality of the area.</p> <p>New development gives regard to the style, form and scale of any primary existing building on the land, and minimises its visual impact within the foreshore setting.</p> <p>New development should meet the following design standards:</p> <ul style="list-style-type: none">Sloped and varied roof lines (including pitches or curves) and building line to reduce the visual bulk of the building.Innovative coastal design themes, including the use of lightweight, glazed and metallic materials, open decks and balconies, curved roofs and refreshing coastal colours.Split level design to better integrate the building into a sloping site and minimise the need for earthworks.Robust coastal building materials, including the sensitive use of horizontal and vertical weatherboard wall cladding, light tone rendered brickwork, Coburned roofs, timber and stainless steel verandahs, doors and windows and painted finishes.Innovative building design techniques and materials that reduce energy use and are responsive to coastal conditions.Avoid direct overlooking into neighbouring properties and minimise the loss of significant views across the site from neighbouring properties; andIn the Port Phillip Bay and Port Lonsdale Road Areas, the use of colour finishes which are non-reflective and of natural tones which blend in with the vegetated coastal setting.	<p>The proposed development is fitting with the existing height and scale of other properties along Point Lonsdale Road. The design incorporates the use of timber adding to the coastal feel of the building.</p> <p>The development encourages cross ventilation in all dwellings, and makes use of the coastal views with east facing balconies for the apartments.</p> <p>The development reflects the varied roof styles in the area in the use of a modern interpretation of the gable with dormer windows for the townhouses, and a sloped roof for the apartments fronting Point Lonsdale Road.</p> <p>The slope of the roof follows the natural slope of Point Lonsdale Road.</p> <p>Timber privacy screens ensure no overlooking into neighbouring properties.</p>
Building Height and Setbacks	<p>The height and setback of new development retain the existing prevailing rhythm, height and setback along both the street and the foreshore.</p> <p>The height and setback of new development ensure that no individual building or addition dominates the streetscape, dune or coastal landscape.</p> <p>The height of new development does not protrude above the ridge line of the dunes, or in particularly sensitive locations, project above the coastal line canopy to form a silhouette against the sky when viewed from the foreshore or any significant public viewing point.</p> <p>The height and setback of new development ensures adequate daylight to all habitable rooms and private open spaces on the land.</p> <p>The siting of new development, where appropriate, provides the opportunity for the foundation of established gardens in front setbacks to contribute to the coastal and streetscape setting.</p> <p>Building height which is not significantly higher than the height of adjoining buildings or other buildings in the street.</p> <p>A front setback which matches the setback of adjoining buildings and where these setbacks are different, a front setback which is between the setbacks of adjoining buildings or equal to the average setback of buildings in the street, whichever is the lesser of the two.</p> <p>A partial second storey / buildings are openly visible from the street.</p> <p>Avoids building across the width of the site, from boundary to boundary.</p>	<p>The proposed development is fitting with the existing height and setbacks of both Point Lonsdale Road and Simpson St.</p> <p>The building does not exceed the height of any adjoining property.</p> <p>The sloping roof of the development fronting Point Lonsdale Road follows the natural slope of the street.</p> <p>The front setbacks align with the adjoining properties.</p>
Fencing, Driveways and Landscaping	<p>Fencing, particularly to the site's front or coastal boundary in the Swan Bay foreshore area, relates strongly to the open and transparent character of this coastal setting.</p> <p>Fencing, particularly along the front of the site, takes into account the open</p>	<p>Vehicle access will be from Simpson St for all dwellings</p>

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Zones			
30	Port Phillip Planning Scheme		
Map 8	The property is located on Map 5 of the Borough of Queenscliffe Planning Scheme. Map 5 indicates that the property is zoned C1Z – Commercial 1 Zone.		
34.01	Commercial Zone 1		
34.01-1	Table of uses.		
	A permit is not required to use the land for the construction of a dwelling. A permit is not required to use the land for the construction of a shop/retail premises/office.		
34.01-4	Building and Works		
	A permit is required to construct a building or construct or carry out works. A development must meet the requirements of Clause 55.		
34.01s	Schedule to the Commercial 1 zone.		
	Minimum street setback to Rescode Clause 55 Standard B6. Building height to Rescode Clause 55 Standard B7. Site coverage to Rescode Clause 55 Standard B8. Side and rear setbacks to Rescode Clause 55 Standard B17. Private open space to Rescode Clause 55 Standard B28. Front fence height to Rescode Clause 55 Standard B32.		
34.01-8	Decision Guidelines		
	Before deciding on an application, in addition to the decision guidelines in Clause 65, the responsible authority must consider, as appropriate:		
General			
-	The State Planning Policy Framework and the Local planning Policy Framework, including the Municipal Strategic Statement and local planning policies.	See 20 Local Planning Policies for written response to 22.04 and 50 Particular Provisions for Clause 55, outlining planning policies	
-	The interface with adjoining zones, especially the relationship with residential areas.	The design respects the interface between the zones, by providing an active street front to the Point Lonsdale commercial precinct and a contemporary townhouse elevation to the Simpson Street residential streetscape.	
Use			
-	The effect that existing uses may have on the proposed use.	The existing use is a double storey shop and rear car park; this will have no impact on the proposed use.	
-	The drainage of the land.	The dual street frontage allows for easy connection to services and drainage.	
-	The availability of and connection to services.	The effect of traffic to the existing roads would be marginal due to the close proximity of facilities, allowing for other transportation modes such as walking or cycling.	
-	The effect of traffic to be generated on roads.		
-	The interim use of those parts of the land not required for the proposed use.	Land not required for either parking, or dwelling will have soft landscaping.	



50

Particular Provisions

Clause

Description

52.06

Purpose

To ensure that car parking is provided in accordance with the State Planning Policy Framework and Local Planning Framework.

To ensure the provision of an appropriate number of car parking spaces having regard to the demand likely to be generated by the activities on the land and the nature of the locality.

To support sustainable transport alternatives to the motor car.

To promote the efficient use of car parking spaces through the consolidation of car parking facilities.

To ensure that car parking does not adversely affect the amenity of the locality.

To ensure that the design and location of car parking is of a high standard, creates a safe environment for users and enables easy and efficient use.

Number of car spaces required

Column A: Apply the standard rate to all zones	Column B: Only applies where specified in a zone in the Parking Overlay	Column C: Car Parking Measure					
Rate	Rate		TH 1	TH 2	AP 1	AP 2	-/X
dwelling							
1 car space required	1	To each one or two bedroom dwelling, plus	1	1			✓
2 car spaces required	2	To each three or more bedroom dwelling (with studios or studios that are separate from a bedroom) plus			2	2	✓
1 car space required	0	For visitors to every 5 dwellings for developments of 5 or more dwellings					X

52.06-5

Column A: Apply the standard rate to all zones

Column B: Only applies where specified in a zone in the Parking Overlay

Column C: Car Parking Measure

Rate	Rate		TH 1	TH 2	AP 1	AP 2	-/X
dwelling							
1 car space required	1	To each one or two bedroom dwelling, plus	1	1			✓
2 car spaces required	2	To each three or more bedroom dwelling (with studios or studios that are separate from a bedroom) plus			2	2	✓
1 car space required	0	For visitors to every 5 dwellings for developments of 5 or more dwellings					X

55

Two or more dwellings on a lot and residential buildings

55.01

Neighbourhood & site description & design response

An application must be accompanied by:

A neighbourhood and site description.

A design response.

55.01-1

Neighbourhood & site description

Context

Location

The site is located in a mixed use area with both residential and commercial uses. It has dual frontage to both Point Lonsdale Road and Simpson Street. Several foreshore reserves and parks are nearby, and are within the neighbourhood activity centre of Point Lonsdale Road. The surrounding neighbourhood is sloped. It rises up Point Lonsdale Road to the south and also to the west up Adams Street.

There are no significant view lines from, over, or to the site.

The neighbourhood is a mixture of residential and commercial buildings ranging in height from 1 to 3 storeys. The surrounding neighbourhood is a diverse mixture of single storey period cottages, post war infill and contemporary infill of varied style and mainly 2 storeys.

The activity centre on Point Lonsdale Road between Kirk Road and Adams Street has higher density development. It is serviced by the Bellarine Highway.

Streetscape

Point Lonsdale Road stretches from the Bellarine Highway to the Point Lonsdale Pier. Point Lonsdale Road differs along its length from single storey period dwellings to 3 storey apartment buildings. The majority of the taller buildings are located at the southern end in the activity centre.

Simpson Street runs between Kirk Road and Adams Street and is parallel to Point Lonsdale Road.

Simpson Street is a mixture of single and double storey dwellings and townhouses.

The residential areas have a mixture of front gardens from medium to large, as well as a variety of front fences. However the activity centre has only a few residential properties with gardens and front fences with the rest having full engagement with the street.

Site Planning

Site shape, size, orientation

Sites run in an east-west orientation, rotated 92 degrees of north.

Sites are irregularly shaped.

Sites vary in width, reflecting an irregular pattern of subdivision.

Street, side and rear setbacks.

Street setbacks are predominately range from zero to 5.0m.

Secondary setback to first floors is not common.

Side setbacks are diverse ranging from zero.

Dwellings built boundary to boundary are common.

Rear setbacks are range from 2 to 10m.

Off street parking is common however most garages are accessed from the street front.

Building height

The right hand adjoining property, 61 Point Lonsdale Rd has a height of approximately 3.9m.

The left hand adjoining property, 51 Point Lonsdale Rd has a height of approximately 9.5m.

The right hand adjoining property, 21 Simpson Street has a height of approximately 7.6m.

The left hand property, 17 Simpson Street has a height of approximately 3.4m.

Buildings in the surrounding area are diverse, ranging from single storey to 2 and 3 storey apartment/ townhouses.

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Impact on adjoining properties
61 Point Lonsdale Road
The property to the north east is a single storey shop, fronting Point Lonsdale Road with a small rear yard.
The main outlook is to the east and west.
There are no habitable windows that face the subject site.
51 Point Lonsdale Rd
The dwelling to the south east is three storeys with a ground floor shop and upper storey apartment configuration, with raised screened private open space (deck) at rear.
The property has frontage to Point Lonsdale Road and Admans Street.
There is car parking to the rear with access from Admans Street, access is shared with 21 Simpson Street.
There are 3 habitable windows that face the subject site.
The main outlook is to the east and west.
17 Simpson Street
The dwelling to the north west is a single storey brick house with small front and rear yard.
There is a garage to the south side of the property with access to Simpson Street.
There is one habitable window that faces the subject site.
21 Simpson Street
The property to the south west is two double storey townhouses, with raised screened private open space (deck) at rear.
There is garage access to the rear with access from Admans Street, access is shared with 51 Point Lonsdale Road.
There are 3 habitable room windows that face the subject site.

Landscape design and layout
Planting on Point Lonsdale Road is not common except for the areas of foreshore reserve.
Simpson Street has mature tree coverage along the nature strip.
Medium to large gardens with informal planting to the front with small to no garden to the rear of the property.
Properties with marginal planting in either front or back are common in this neighbourhood.
Cross over locations, parking and carport locations.
There are only a few cross overs on Point Lonsdale Rd.
Cross overs are common in Simpson Street, which runs parallel to Point Lonsdale Rd.
Walls on boundaries.
Walls on boundaries are common to the area.
51 Point Lonsdale Road has a 2 storey wall on boundary
67 Point Lonsdale Road has a 3 storey wall on both boundaries
71 Point Lonsdale Road has a 2 storey wall on boundary
77 Point Lonsdale Road has a 3 storey wall on both boundaries
79 Point Lonsdale Road has a 2 storey wall on boundary
81 Point Lonsdale Road has a 2 storey wall on both boundaries
88-97 Point Lonsdale Road has a 2 storey wall on both boundaries
Verandas, eaves and parapets.
Verandas are common to the area.
Eaves are typically short with little overhang.
Contemporary infill has little to no eave overhang.
Parapets are more common in contemporary dwellings than the older stock.
Façade articulation
There is articulation in the form of dual setbacks, overhanging elements and recessed entries.
Front fences.
Front fences are varied in type and height.
Point Lonsdale Road is predominately no front fence except for a few timber fences to the townhouses.
Simpson street varies in fence heights from no fence to 2m. The material is mostly timber.

Site	
<i>Site shape, size, orientation & easements</i>	
The site area is 465m ² .	
The site is orientated East West, rotated 92 degrees of north.	
There are 5 easements on the site.	
Easements 4 and 5 contain private assets connecting 21 Simpson St to the sewer main.	
Levels	
The site is sloped 1.0m from West to East	
It also has a cross slope of 0.4m from north to South.	
The adjoining properties have similar levels.	
Existing dwelling	
The existing dwelling on the site is a 2 storey brick and cement sheet dwelling with a corrugated metal roof and has a zero set back from the front boundary.	

	<p>Context</p> <p>Streetscape Cafe and dwellings set towards front and private open space at the rear, with car parking access from Simpson Street No front fence No front garden 2.5m setback from Point Lonsdale Road. Three storey brick, with each upper level set back from the street. 3 storey wall on boundary Metal geometric roof. Contemporary cafe on ground floor with upper storey apartments. Large window openings, with glass a main feature. Car access to rear from Simpson St.</p> <p>67 Point Lonsdale Rd</p>
	<p>Context</p> <p>Streetscape Set of townhouses all next to each other. Rear shared central courtyard, garage with car access from Point Lonsdale Rd. 1.5m timber picket fence. Small front garden with informal planting Setback 5.0m from Point Lonsdale Rd. Two storey townhouse. Tile gable roof. Contemporary brick townhouse block with timber feature inserts. Large window openings Car access to front garage from Point Lonsdale Rd.</p> <p>71 Point Lonsdale Rd</p>
	<p>Context</p> <p>Streetscape Shop and dwellings set towards front, with townhouses to rear and car access from Simpson Street. No front fence. No front garden. Setback 0m from street. Three storey with upper levels set back from the street. 2 storey wall on boundary Metal sloped roof. Contemporary rendered brick and weatherboard ground floor shop and upper storey apartments Large window openings Car access from Simpson Street</p> <p>89-97 Point Lonsdale Rd</p>






Point Lonsdale Rd

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	<p>Context</p> <p>Streetscape Shop and dwellings set towards front with townhouses to rear and car access from side. No front fence. No front garden. Set back 0m from Point Lonsdale Road. Three storey with first floor set back from boundary. Corrugated metal hipped roof. Contemporary shop on ground with upper storey apartments. Large window openings. Car access from Adams street via shared driveway.</p> <p>51 Point Lonsdale Rd</p>
	<p>Context</p> <p>Streetscape Shop set towards front with small rear yard. No front fence. No front garden. Set back 0.5m from street. Single storey shop. Metal flat roof. Contemporary shop with large awning. Large front display window. No car access.</p> <p>61 Point Lonsdale Rd</p>
	<p>Context</p> <p>Streetscape Shop set towards front and private open space at the rear. No front fence. No front garden. Setback 0.5m from street. Single storey, weatherboard shop. Metal sloped roof. Post war weatherboard shop with deep feature eaves and minimal features. Large window openings. No car access.</p> <p>63 Point Lonsdale Rd</p>

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	<p>Context</p> <p>Streetscape</p> <p>Townhouses set in rear of 88-97 Point Lonsdale Rd, car access to rear parking.</p> <p>1.8m high timber fence.</p> <p>Small front garden with minimal planting.</p> <p>No planting in the road reserve.</p> <p>Setback 4.2m from street.</p> <p>Two storey townhouses.</p> <p>Metal sloped/ flat roof.</p> <p>Contemporary rendered brick and weatherboard townhouses.</p> <p>Large window openings with some timber screening.</p> <p>Car access from Simpson street.</p>
	<p>Context</p> <p>Streetscape</p> <p>House set towards front with private open space to the side and rear.</p> <p>1.0m high open timber fence.</p> <p>Large front garden with informal planting and some mature trees.</p> <p>Established native coastal planting in the road reserve.</p> <p>10.0m setback from street.</p> <p>Two storey dwelling.</p> <p>Metal gable roof.</p> <p>Post war weatherboard dwelling with timber features.</p> <p>Timber sash windows.</p> <p>Car access to side carcase from Simpson</p> <p>4 Adams Street</p>
	<p>Context</p> <p>Streetscape</p> <p>House set towards front with private open space to the side and rear.</p> <p>1.0m high timber and wire fence.</p> <p>Large front garden with informal planting and mature trees.</p> <p>Established native coastal planting in the road reserve.</p> <p>11.0m setback from street.</p> <p>Single storey dwelling.</p> <p>Tile hipped roof.</p> <p>Period weatherboard dwelling with timber features.</p> <p>Timber sash windows.</p> <p>Car access to side garage from Simpson Street.</p> <p>16 Simpson Street</p>

	<p>Context</p> <p>Streetscape</p> <p>Townhouses set in rear of 51 Point Lonsdale Rd, entry from shared driveway on Adams Street.</p> <p>2.1m timber paling fence.</p> <p>Established native coastal planting in the road reserve.</p> <p>Two storey townhouses.</p> <p>Corrugated metal hipped roof.</p> <p>Contemporary weatherboard dwellings.</p> <p>Vertical proportioned windows.</p> <p>Car access from Adams street via shared driveway</p>
	<p>Context</p> <p>Streetscape</p> <p>House set towards front with garage and car access to the side and private open space to the rear.</p> <p>1.8m high timber fence.</p> <p>Established native coastal planting in the road reserve.</p> <p>5.9m setback from street.</p> <p>Single storey dwelling.</p> <p>Metal flat roof.</p> <p>Post war cement sheet dwelling.</p> <p>Symmetrical windows to front facade.</p> <p>Car access to garage from Simpson street.</p>
	<p>Context</p> <p>Streetscape</p> <p>Set of townhouses all next to each other.</p> <p>Rear set of 71 Point Lonsdale Rd</p> <p>1.5m high timber paling fence.</p> <p>Established planting in the road reserve.</p> <p>Setback 5.0m from street.</p> <p>Two storey townhouses.</p> <p>Tile gable roof.</p> <p>Contemporary brick townhouse block with timber feature inserts.</p> <p>Large window openings.</p> <p>Car access from Simpson St to shared central parking area set between the two fronts of townhouses.</p>



Context
Adjoining properties
6.9m high wall on boundary with garage door access.
3 habitable windows face the subject site.
First floor private open space.
Car parking at rear of property, shared with 21 Simpson Street.



Context
Adjoining properties
2.8m high wall on boundary.
One habitable window is facing the subject site.
Private open space to the side and rear with planting.
Car access to garage from Simpson Street.



Context
Adjoining properties
3.5m high wall on boundary.
Three habitable windows face the subject site.
First floor private open space.
Car parking at rear of property, shared with 51 Point Lonsdale Rd.

51 Point Lonsdale Road

21 Simpson Street

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Context
Site
9.88 m frontage with shop set towards front and private open space at the rear.
No front fence.
No front garden.
Setback 0m from street.
Two storey shop and dwelling.
Corrugated metal hipped roof.
Contemporary brick and cement sheet shop and dwelling.
Large window openings and minimal features.
Car access from Simpson street.



Context
Rear private open space
Private open space has a grass covering to perimeter and central driveway, out buildings to either side.
2.1m high paling fence at side boundaries and no fence at rear boundary.
Access to Simpson Street.



Context
Adjoining properties
4.8m high at highest point wall on boundary adjoining 61 Point Lonsdale Rd.
No habitable windows are facing the subject site.
Small private open space to rear of the shop.

57-59 Point Lonsdale Road

61 Point Lonsdale Road

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<p>Streetscape</p> <p>The height of the development does not exceed any existing buildings on Point Lonsdale Road or Simpson St. The first and second floor balconies are aligned with neighbouring properties' balconies, taking into account the natural slope of the street.</p> <p>This allows the proposed development to sit well with the other buildings in the street.</p> <p>The retail space on the ground floor fronting Point Lonsdale Road is similar in height to the other retail spaces along the street, continuing the existing scale and form of other similar developments on Point Lonsdale Road.</p>	 <p>View of second floor apartment open plan living area and roof deck overlooking Point Lonsdale</p>	 <p>View of townhouse living area</p>
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Clause	Description	Design response	Assessments
55.01-2	<p>Accommodation</p> <p>The proposal is for 4 dwellings, including 2 townhouses, 2 apartments and one retail front. Constructed as two separate developments set one in front of the other with the townhouses fronting Simpson street and the shop and apartments fronting Point Lonsdale Road.</p> <p>The development fronting Point Lonsdale Road comprises of a ground floor retail front, one first floor apartment and one second floor apartment with roof deck.</p> <p>The ground floor contains the retail premises, car parking to the rear and entry to the communal stair well and lift.</p> <p>The first floor apartment contains 4 bedrooms, 2 bathrooms, laundry and an open plan living, kitchen, dining area leading out onto a private deck overlooking Point Lonsdale.</p> <p>The second floor apartment is identical to the first floor, with an added roof deck above.</p> <p>The development fronting Simpson Street comprises of 2, three storey townhouses.</p> <p>On the ground floor, each townhouse comprises a parking space, a private entry, laundry, 1 bedroom and 1 bathroom. The first floor of the townhouses is an open plan living, dining, kitchen area with a private screened balcony, and the second floor contains an open plan combined bedroom and living space, a bathroom, and screened balcony.</p> <p>Similar existing building types</p> <p>Point Lonsdale has a dominant presence of post war and period weatherboards, and contemporary single and multi-storey developments that incorporate a variety of materials.</p> <p>The readily identifiable elements of gable roofs, varied roof styles, and the use of textural materials can be reinterpreted into contemporary design.</p> <p>Contemporary multi-residential developments similar to the proposal are common to Point Lonsdale Road. These have a similar height and form with a mix of sloped, gabled and curved roofs.</p> <p>The proposal reflects the existing buildings in the continuing use of timber screens and balconies overlooking Point Lonsdale from the first floor.</p>	 <p>Example of retail on ground floor, apartments at first floor 41 Highbrook Ave Berwon Heads</p>  <p>Timber screen balconies</p>	 <p>Example of sloped roof</p>



Proposed streetscape from north east.



Proposed streetscape from south east.

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Existing streetscape from north east.



Existing streetscape from south east.

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Existing streetscape from south west.



Proposed streetscape from south west.



Existing streetscape from north west.



Proposed streetscape from north west.

Site planning
The site planning follows that of the street, with the building mass and both street frontages aligning with the adjoining properties.

Building massing
The development has two clearly defined elements.
The apartments align with the adjoining shop fronts and stepping back at first and second floors in alignment with 51 Point Lonsdale Road.
At the rear it aligns with the rear of 51 Point Lonsdale Road.
The townhouses align with the setback of 21 Simpson St.
The ground floor is partly lowered and the second floor incorporated into an attic style roof form.
The two elements are separated by open space over the parking area.

Landscape
The townhouses have planting opportunities with a proposed garden bed with canopy trees close to Simpson St, and another at the rear of the townhouses.
Significant view lines
There are no significant viewlines affected.



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Built Form
The built form comprises a number of easily recognisable elements, positioned to integrate with the streetscape and minimise the impact on adjoining properties.

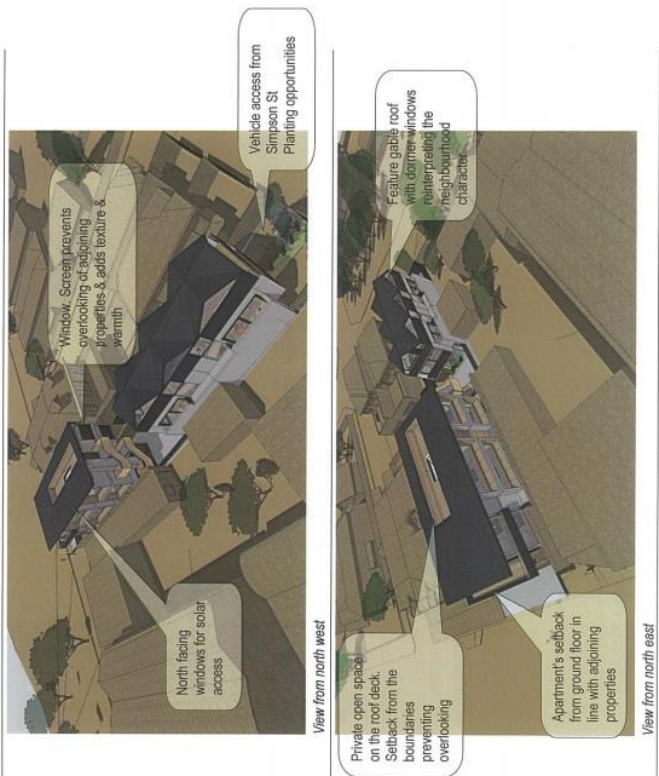
Ground floor
Ground floor is setback 0.0m fronting Point Lonsdale Road, and 5.0m from Simpson St, averaging the setbacks of existing properties on either side.
It is identifiable through the expression of concrete and glazing.

First floor
The first floor is expressed in concrete and some windows have timber screens.
Timber as a material is common to the area, and timber screens are common on other properties in Point Lonsdale Road.
The heights relate to the surrounding properties and follow the natural slope of the land on Point Lonsdale road

Second floor
The second floor is expressed in metal cladding for the townhouses, creating a contrast of materials and introducing another texture.
The apartments second floor is expressed in concrete and metal cladding to balconies.
Timber screens are used on the balconies and north facing windows.
The use of timber creates a contrast to the concrete.

Gable roof
Gable roofs are common to the area.
The townhouses incorporate a modern interpretation assimilating to the neighbourhood character..

Screens
All balconies and several windows have a horizontal screen that prevents views into the adjoining properties while adding warmth and texture to the overall composition.



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55.02-5	Integration with the street objective To integrate the layout of development with the street.	
	Standard B5	
	Developments should provide adequate vehicle and pedestrian access to and from the street. Development should be oriented to front existing and proposed streets. High fencing in front of dwellings should be avoided if practicable. Development need to existing public open spaces should be laid out to complement the open space.	The townhouses front Simpson St and the apartments front Point Lonsdale Road. Pedestrian access via a gate is accessed from both roads; car access for all dwellings is from Simpson St. The proposed development complies with Standard B5.
55.03	Site layout and building massing	
55.03-1	Street setback objective To ensure that the setbacks of buildings from a street respect the existing or preferred neighbourhood character and make efficient use of the site.	
	Standard B6	
	Walls of buildings should be setback from streets: - If no distance is specified in the schedule to the zone, the distance specified in Table A1 Porches, pergolas and verandahs that are less than 3.6 metres high and eaves may encroach not more than 2.5 metres into the setbacks of this standard.	The adjoining setbacks are 0.0m at 61 Point Lonsdale Road, and 0.5m at 51 Point Lonsdale Road. The setback of 21 Simpson St is 3.4m and 17 Simpson St has a setback of 6m The proposed developments fronting both Point Lonsdale Road and Simpson St, match the adjoining properties' setbacks. The proposed development complies with Standard B6.
55.03-2	Building height objective To ensure that the height of buildings respects the existing or preferred neighbourhood character.	
	Standard B7	
	The maximum building height should not exceed: - If no maximum height is specified in the schedule to the zone, 9 metres. Changes of building height between existing buildings and new buildings should be graduated.	The maximum height of the proposed development fronting Point Lonsdale Road is. The max height of the development fronting Simpson St is 9.5m This does not exceed any existing dwelling on either Point Lonsdale Road or Simpson St. The proposed development complies with Standard B7.
55.03-3	Site coverage objective To ensure that the site coverage respects the existing or preferred neighbourhood character and responds to the features of the site.	
	Standard B8	
	The site area covered by buildings should not exceed: - If no maximum site coverage is specified in the schedule to the zone, 60 per cent.	The neighbouring site coverage varies between 83% at 61 Point Lonsdale Road, and 81% at 51 Point Lonsdale Road. The proposed development has site coverage of 69% and respects the neighbourhood character and meets the objective. The site area is 465m ² . The existing site coverage 158m ² , OR 34%. The proposed site coverage is 320m ² , OR 69%. The proposed development does not comply with B8.
55.03-4	Permeability objectives To reduce the impact of increased stormwater run-off on the drainage system. To facilitate on-site stormwater infiltration.	
	Standard B9	
	At least 20 per cent of the site should not be covered by impervious surfaces.	The existing permeable area is 307m ² or 66%. The proposed permeable area is 15m ² OR 3%. The proposed development does not comply with standard A6

Clause	Description	Assessments
55.02	Neighbourhood character	
55.02-1	Neighbourhood character objective To ensure that the design respects the existing neighbourhood character or contributes to a preferred neighbourhood character. To ensure that the design responds to the features of the site and the surrounding area.	
	Standard B1 The design response must be appropriate to the neighbourhood and the site. The proposed design must respect the existing or preferred neighbourhood character and respond to the features of the site.	The proposed development complies with Standard B1.
55.02-2	Residential policy objective To ensure that residential development is provided in accordance with any policy for housing in the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies. To support medium densities in areas where development can take advantage of public transport and community infrastructure and services.	
	Standard B2 An application must be accompanied by a written statement to the satisfaction of the responsible authority that describes how the development is consistent with any relevant policy for housing in the State Planning Policy Framework and the Local Planning Policy Framework, including the Municipal Strategic Statement and local planning policies.	The proposed development complies with Standard B2.
55.02-3	Dwelling diversity objective To encourage a range of dwelling sizes and types in developments of ten or more dwellings.	
	Standard B3 Developments of ten or more dwellings should provide a range of dwelling sizes and types, including: - Dwellings with a different number of bedrooms. - At least one dwelling that contains a kitchen, bath or shower, and a toilet and wash basin at ground floor level.	Four dwellings, not applicable. The proposed dwelling complies with Standard B3.
55.02-4	Infrastructure objective To ensure development is provided with appropriate utility services and infrastructure. To ensure development does not unreasonably overload the capacity of utility services and infrastructure.	
	Standard B4 Development should be connected to reticulated services, including reticulated sewerage, drainage, electricity and gas, if available. Development should not unreasonably exceed the capacity of utility services and infrastructure, including reticulated services and roads. In areas where utility services or infrastructure have little or no spare capacity, developments should provide for the upgrading or mitigation of the impact on services or infrastructure.	The site is well serviced by utilities, infrastructure and public transport. The proposed dwellings comply with Standard B4.



55.03-5	Energy efficiency protection objectives To achieve and protect energy efficient dwellings and residential buildings. To ensure the orientation and layout of development reduce fossil fuel energy use and make appropriate use of daylight and solar energy.	Standard B10 Buildings should be: - Oriented to make appropriate use of solar energy. - Sited and designed to ensure that the energy efficiency of existing dwellings on adjoining lots is not unreasonably reduced. Living areas and private open space should be located on the north side of the development, if practicable. Dwellings should be designed so that solar access to north facing windows is maximised.	Townhouse living areas are orientated toward the north, and east and west, depending on the townhouse. Apartment living areas are orientated towards the north and east. Private open space is located on the balconies and roof deck. The townhouse living/ kitchen open plan area faces north, and east and west depending on the townhouse, and looks out onto the private open space on the screened balconies. The apartment living/ kitchen open plan area faces north and east and looks out to the private open space on the screened balconies on the east. Townhouse bedrooms face east and west depending on the townhouse. Apartment bedrooms face north, east and west. The energy efficiency of dwellings on adjoining lots is not impacted. The proposed development complies with Standard B10.
55.03-6	Open space objective To integrate the layout of development with any public and communal open space provided in or adjacent to the development.	Standard B11 If any public or communal open space is provided on site, it should: - Be substantially fronted by dwellings, where appropriate. - Provide outlook for as many dwellings as practicable. - Be designed to protect any natural features on the site. - Be accessible and usable.	The proposed development complies with Standard B11.
55.03-7	Safety objective To ensure the layout of development provides for the safety and security of residents and property.	Standard B12 Entrances to dwellings and residential buildings should not be obscured or isolated from the street and internal accessways. Planting which creates unsafe spaces along streets and accessways should be avoided. Developments should be designed to provide good lighting, visibility and surveillance of car parks and internal accessways. - Private spaces within developments should be protected from inappropriate use as public thoroughfares.	The proposed development complies with Standard B12.
55.03-8	Landscaping objective	Standard B13 The landscape layout and design should: - Protect any predominant landscape features of the neighbourhood. - Take into account the soil type and drainage patterns of site. - Allow for intended vegetation growth and structural protection of buildings. - In locations of habitat importance, maintain existing habitat and provide for new habitat for plants and animals. - Provide a safe, attractive and functional environment for residents. Development should provide for the retention or planting of trees, where these are part of the character of the neighbourhood. Access objective To ensure vehicle access to and from a development is safe, manageable and convenient. To ensure the number and design of vehicle crossovers respects the neighbourhood character.	2 proposed garden beds to include canopy trees and other planting. The proposed development complies with Standard B13.
55.03-9	Access objective To ensure vehicle access to and from a development is safe, manageable and convenient. To ensure the number and design of vehicle crossovers respects the neighbourhood character.	Standard B14 Accessways should: - Be designed to allow convenient, safe and efficient vehicle movements and connections within the development and to the street network. The width of accessways or car spaces should not exceed: - 33 per cent of the street frontage, or - If the width of the street frontage is less than 20 metres, 40 per cent of the street frontage. No more than one single-width crossover should be provided for each dwelling fronting a street. The location of crossovers should maximise the retention of on-street car parking spaces.	Vehicle access is clearly defined from Simpson St. The proposed development complies with Standard B14.
55.03-10	Parking location objective To provide convenient parking for resident and visitor vehicles. To add parking and traffic facilities in the development and the neighbourhood. To protect residents from vehicular noise within developments.	Standard B15 Car parking facilities should: - Be reasonably close and convenient to dwellings and residential buildings. - Be secure. - Be designed to allow safe and efficient movements within the development. - Be well ventilated if enclosed. Large parking areas should be broken up with trees, buildings or different surface treatments. Shared accessways or car parks of other dwellings and residential buildings should be located at least 1.5 metres from the windows of habitable rooms. This setback may be reduced to 1 metre where there is a fence at least 1.5 metres high or where window sills are at least 1.4 metres above the accessway.	There is one shared driveway accessed from Simpson St. The two townhouses each have a single carspace. The two apartments each have 2 car spaces. There is a single visitor's car space. The shared driveway is not located adjacent to any habitable room windows. The proposed development complies with Standard B15.



55.04	Amenity impacts																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																		</
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55.04-5	Overshadowing open space objective To ensure buildings do not significantly overshadow existing secluded private open space.	Standard B21 Where sunlight to the secluded private open space of an existing dwelling is reduced, at least 75 percent, or 40 square metres with minimum dimension of 3 metres, whichever is the lesser area, of secluded private open space should receive a minimum of 5 hours of sunlight between 9 am and 3 pm on 22 September. If existing sunlight to the secluded private open space of an existing dwelling is less than the requirements of this standard, the amount of sunlight should not be further reduced.	<table><thead><tr><th>Existing light</th><th>Proposed light</th><th>B21 requirement</th><th>✓/X</th></tr></thead><tbody><tr><td>9am</td><td>24 m²</td><td>20m²</td><td>40m² 83%</td></tr><tr><td>12noon</td><td>23 m²</td><td>23m²</td><td>40m² 100%</td></tr><tr><td>3pm</td><td>4m²</td><td>4m²</td><td>40m² 100%</td></tr></tbody></table> <p>The total area of the existing secluded private open space to 51 Point Lonsdale Road is 27m².</p>	Existing light	Proposed light	B21 requirement	✓/X	9am	24 m ²	20m ²	40m ² 83%	12noon	23 m ²	23m ²	40m ² 100%	3pm	4m ²	4m ²	40m ² 100%
Existing light	Proposed light	B21 requirement	✓/X																
9am	24 m ²	20m ²	40m ² 83%																
12noon	23 m ²	23m ²	40m ² 100%																
3pm	4m ²	4m ²	40m ² 100%																
55.04-6	Overlooking objective To limit views into existing secluded private open space and habitable room windows.	Standard B22 A habitable room window, balcony, terrace, deck or patio should be located and designed to avoid direct views into the secluded private open space of an existing dwelling within a horizontal distance of 9 metres (measured at ground level) of the window, balcony, terrace, deck or patio. Views should be measured within a 45 degree angle from the plane of the window or perimeter of the balcony, terrace, deck or patio, and from a height of 1.7 metres above floor level. A habitable room window, balcony, terrace, deck or patio with a direct view into a habitable room window of an existing dwelling within a horizontal distance of 9 metres (measured at ground level) of the window, balcony, terrace, deck or patio should be either: - Have permanently fixed external screens to at least 1.7 metres above floor level and be no more than 25 percent transparent. Screens used to obscure a view should be: - Perforated panels or trellis with a maximum of 25 per cent openings or solid translucent panels. - Permanent, fixed and durable. Designed and coloured to blend in with the development.	<table><thead><tr><th>Existing light</th><th>Proposed light</th><th>B21 requirement</th><th>✓/X</th></tr></thead><tbody><tr><td>9am</td><td>4 m²</td><td>1m²</td><td>40m² 25%</td></tr><tr><td>12noon</td><td>23 m²</td><td>22m²</td><td>40m² 100%</td></tr><tr><td>3pm</td><td>18m²</td><td>18m²</td><td>40m² 100%</td></tr></tbody></table> <p>The proposed development complies with Standard B21.</p>	Existing light	Proposed light	B21 requirement	✓/X	9am	4 m ²	1m ²	40m ² 25%	12noon	23 m ²	22m ²	40m ² 100%	3pm	18m ²	18m ²	40m ² 100%
Existing light	Proposed light	B21 requirement	✓/X																
9am	4 m ²	1m ²	40m ² 25%																
12noon	23 m ²	22m ²	40m ² 100%																
3pm	18m ²	18m ²	40m ² 100%																
55.04-7	Internal views objective To limit views into the secluded private open space and habitable room windows of dwellings and residential buildings within a development.	Standard B23 Windows and balconies should be designed to prevent overlooking of more than 50 per cent of the secluded private open space of a lower-level dwelling or residential building directly below and within the same development.																	
55.04-8	Noise impacts objective William, James and Kate Casserly 3/53 Stewart St (po box 2392) Richmond 3121 phone 9428 1415 fax 9428 1340 mail@threecarchitects.com.au ABN: 73 145 834 993																		





55.05-3	<ul style="list-style-type: none"> - The maximum height specified in the schedule to the zone, or - If no maximum height is specified in the schedule to the zone, the maximum height specified in Table B3. 	There is a transparent timber gate fronting Point Lonsdale Road. The proposed development complies with Standard B32.
	Common property objective	
	To ensure that communal open space, car parking, access areas and site facilities are practical, attractive and easily maintained. To avoid future management difficulties in areas of common ownership.	
	Standard B33 Developments should clearly delineate public, communal and private areas. Common property, where provided, should be functional and capable of efficient management.	The common driveway and carpark will be functional and easily maintained. The proposed development complies with Standard B33.
55.05-4	Site services objective	
	To ensure that site services can be installed and easily maintained.	
	To ensure that site facilities are accessible, adequate and attractive.	
	Standard B34 The design and layout of dwellings and residential buildings should provide sufficient space (including easements where required) and facilities for services to be installed and maintained efficiently and economically. Bin and recycling enclosures, mailboxes and other site facilities should be adequate in size, durable, waterproof and blend in with the development. Bin and recycling enclosures should be located for convenient access by residents. Mailboxes should be provided and located for convenient access as required by Australia Post.	The dwellings will have individual services easily accessed and maintained. The proposed development complies with Standard B34.

55.05-4	Private open space objective	
	To provide adequate private open space for the reasonable recreation and service needs of residents.	
	Standard B28 A dwelling or residential building should have private open space of an area and dimensions specified in the schedule to the zone, a balcony. The first floor apartment has 20m ² of private open space in the form of a balcony and roof deck. The second floor apartment has 50m ² of private open space in the form of a balcony and roof deck. The proposed development complies with Standard B28.	Both townhouses have 12.4m ² of private open space in the form of 3 balconies each. The first floor apartment has 20m ² of private open space in the form of a balcony and roof deck. The second floor apartment has 50m ² of private open space in the form of a balcony and roof deck. The proposed development complies with Standard B28.
	<ul style="list-style-type: none"> - An area of 40 square metres, with one part of the private open space to consist of secluded private open space at the side or rear of the dwelling or residential building with a minimum area of 25 square metres, a minimum dimension of 3 metres and convenient access from a living room, or - A balcony of 8 square metres with a minimum width of 1.6 metres and convenient access from a living room, or - A rooftop area of 10 square metres with a minimum width of 2 metres and convenient access from a living room. 	
55.05-5	Solar access to open space objective	
	To allow solar access into the secluded private open space of new dwellings and residential buildings.	
	Standard B29 The private open space should be located on the north side of the dwelling or residential building, if appropriate. The southern boundary of secluded private open space should be set back from any wall on the north of the space at least (2 + 0.3H) metres, where H is the height of the wall.	The balconies and roof deck are served by solar access. The proposed development complies with Standard B29.
	Storage objective To provide adequate storage facilities for each dwelling.	
55.05-6	Standard B30 Each dwelling should have convenient access to at least 6 cubic metres of externally accessible, secure storage space.	All dwellings are provided with 6m ³ of secure storage space as shown on the plans. The proposed development complies with Standard B30.
	Detailed design	
	Design detail objective To encourage design detail that respects the existing or preferred neighbourhood character.	Refer to the Design Response. The proposed development complies with Standard B31.
	Standard B31 The design of buildings, including: - Facade articulation and detailing, - Window and door proportions, - Roof form, and - Verandahs, eaves and parapets, Should respect the existing or preferred neighbourhood character. Garages and carports should be visually compatible with the development and the existing or preferred neighbourhood character.	
55.05-2	Front fences objective	
	To encourage front fence design that respects the existing or preferred neighbourhood character.	
	Standard B32 The design of front fences should complement the design of the dwelling and any front fences on adjoining properties. A front fence within 3 metres of a street should not exceed:	There is no fence fronting Simpson St. A transparent timber gate is setback 6.0m from Simpson St for pedestrian and vehicle access.



SDAPP Sustainable Design Assessment			
Clause	Description	Assessments	✓ / ✗
1.0	Indoor Environment Quality		
Objective			
To achieve a healthy indoor environment quality for the wellbeing of building occupants.			
1.1	Daylight	<p>The proposed townhouses have windows facing north, south, east and west depending on the townhouse, serviced by an abundant natural light in all habitable rooms.</p> <p>This condition minimises the need for artificial heating.</p> <p>Both apartments have predominantly north, east and west facing windows serviced by an abundant natural light in all habitable rooms.</p> <p>This condition minimises the need for artificial heating.</p> <p>It is also worth noting that the provision of daylight in living and working spaces reduces energy consumption. This is due to a reduction in the use of air conditioning, associated with the heat generated by artificial lighting. However, in office environments, daylight has to be carefully balanced with possible glare as this strains our eyes.</p>	✓
1.2	Ventilation	<p>Whether via natural or mechanical means, Council recommends substantially exceeding minimum requirements for window opening sizes and air exchange rates under the Building Code of Australia.</p>	✓
1.3	External Views	<p>The provision of long distance views and a visual connection to the outdoors commonly increases wellbeing for building occupants. In office buildings, views can reduce eyestrain for computer workers; in residential buildings, views provide a sense of connectivity. Due to statutory planning provisions, the balance between sufficient external views and limiting the overlooking into neighbouring properties needs to be carefully considered.</p>	✓
1.4	Volatile organic Compounds	Low VOC paints, carpets and joinery will be used throughout.	✓
2.0	Energy efficiency		
Objective			
To ensure the efficient use of energy, to reduce total operating greenhouse emissions and to reduce energy peak demand.			
2.1	Building fabric	<p>Townhouses</p> <p>The ground floor and first floor walls are concrete with insulated walls.</p> <p>The second floor walls are insulated walls with metal external cladding.</p> <p>Apartments</p> <p>The ground floor, first floor, and second floor walls</p>	✓
2.2	External shading devices	<p>are mostly concrete with insulated walls.</p> <p>To the north east and west, the buildings have some externally screened windows.</p> <p>Townhouses</p> <p>On the ground floor, the bedrooms have full height windows. These receive shading from the overhang from the first floor.</p> <p>On the first floor the open plan kitchen, meals and living area have full height windows and a sliding door.</p> <p>These give access to the decks and receive shading from the overhang of the second floor.</p> <p>On the second floor, bedrooms have high windows to the north and south and full height windows to the east or west depending on the townhouse. The balconies are all timber screened.</p> <p>Apartments</p> <p>On the first floor the apartment has an open plan kitchen, meals and living with full height windows and sliding doors to the north and east.</p> <p>The doors lead out onto the screened deck, shading provided by the floor above overhang.</p> <p>On the second floor the apartment has an open plan kitchen, meals and living with full height windows and sliding doors to the north and east leading onto the screened deck. Shading is provided by the overhang of the roof above.</p>	✓
2.3	Heating system	<p>Choosing the correct heating and/or cooling system for your project can have a big impact on your energy consumption and greenhouse gas emissions.</p> <p>Smart buildings and renovations, will incorporate passive solar design in their construction. This may include above standard insulation, external shading that can be adjusted to changing climate conditions, good cross ventilation and the best possible building orientation.</p> <p>When all of these are combined, buildings will have significantly lower heating requirements in winter and you may be able to avoid fuelled systems.</p>	✓
2.4	Cooling system	Screening of glass minimises the need for cooling.	✓
2.5	Hot water system	<p>On average, hot water heating contributes 20 per cent of a household's energy bill. Choosing a highly efficient system reduces energy costs and a building's environmental footprint. Water heaters follow similar guidelines to heating and cooling devices: Gas fuelled systems are commonly more efficient than electrically fuelled systems.</p> <p>The efficiency of a hot water system is rated in stars. The more the better.</p> <p>A solar hot water system provides a large proportion of the energy required to heat water by directly using the sun's energy. Where possible, supplement outstanding energy requirements using a gas booster, known as a gas boosted solar hot water system.</p> <p>Instantaneous systems are generally more efficient than storage systems as there is no water being stored and constantly reheated. However, this can vary, depending on the fuel source, be it</p>	✓



Point Lonsdale Rd



2.6	renewable energy, electricity or gas. Clothes drying facility	A well designed residential dwelling should incorporate natural clothes drying facilities. External drying spaces give the occupant the opportunity to use sun and wind to dry clothes rather than electrical appliances. Apartment buildings could provide retractable drying racks on individual balconies or a common clothes line on the building's roof terrace.	Balconies are provided for each dwelling. External drying spaces are provided.	✓
2.7	Lighting	When considering artificial lighting design, weigh up these design options: Ensure living areas, including kitchen and bedrooms, do not require any artificial lighting throughout the day. Familiarise yourself with appropriate lighting requirements (how bright is bright enough?) for different living and working spaces. Choose energy efficient lights and lamps. Install efficient lighting controls such as motion, sound and daylight sensors and time switches, or a master switch that controls several lights, and possibly appliances, at once.	Every space has natural light minimising artificial light throughout the day	✓
2.8	Renewable energy		The proposed screened windows, insulated walls and roof and cross ventilation shall reduce energy demands. However no renewable energy systems have been implemented.	x
3.0	Water efficiency			
	Objective	To ensure the efficient use of water, to reduce total operating potable water use and to encourage the appropriate use of alternative water sources.		
3.1	Water efficient showerheads		The new works shall incorporate water efficient shower heads with a minimum 3 star WELS	✓
3.2	Water efficient tap ware		The new works shall incorporate water efficient tap ware with a minimum 5 star WELS	✓
3.3	Water efficient toilets		The new works shall incorporate water efficient toilets with a minimum 4 star WELS	✓
3.4	Rainwater tanks		No water tanks have been incorporated	x
3.5	Water efficient landscaping		The gardens are modest in their planting and water use.	✓
3.3	Grey water reuse		No grey water system has been incorporated.	x
4.0	Stormwater management			
	Objective	To reduce the impact of stormwater run-off, to improve the water quality of stormwater run-off, to achieve best practice stormwater quality outcomes and to incorporate the use of water sensitive		

4.1	urban design, including rainwater re-use. Site area	The total site area is 465m ²	✓
4.2	Impervious surfaces	Porous paving allows rainwater to pass through the pavers and soak into the ground, unlike standard concrete or block pavers. By using porous pavers you can: Reduce the amount of 'impervious' surfaces on your block Increase groundwater recharge by allowing the water to soak through the soil Improve stormwater quality by filtering stormwater and reducing pollutant loads Reduce high flows during peak rain events entering the waterway from urban areas causing stream erosion and habitat scouring. Porous paving is installed in the same way as traditional paving and is available in many forms. They can be used to replace asphalt, concrete or other impervious pavers.	✓
4.3	Previous Surfaces	Garden and decks of 109m ²	✓
4.4	STORM calculation		x
5.0	Building Materials		
	Objective	To minimise the environmental impacts of materials used by encouraging the use of materials with a favourable lifecycle assessment	
5.1	Retention of existing structure	Existing structure is to be demolished.	x
5.2	Environmentally friendly materials	The new works shall specify timber products from renewable sources.	✓
5.3	Specifying recycled materials	Where practical, specifying recycled materials is a more sustainable practice than compared to specifying new materials. Not only will the materials life span be extended and in turn reduce the amount of waste going to landfill, it can also be used as a key design feature of your project. For example, recycled timber can often be reused as new flooring and decking.	x
6.0	Transport		
	Objective	To minimise car dependency and to ensure that the built environment is designed to promote the use of public transport, walking and cycling.	
6.1	Bike storage facilities	Bikes can be stored in the car park, or for the townhouses in the storage areas provided on the ground floor, and for the apartments at the entrance on each floor.	✓



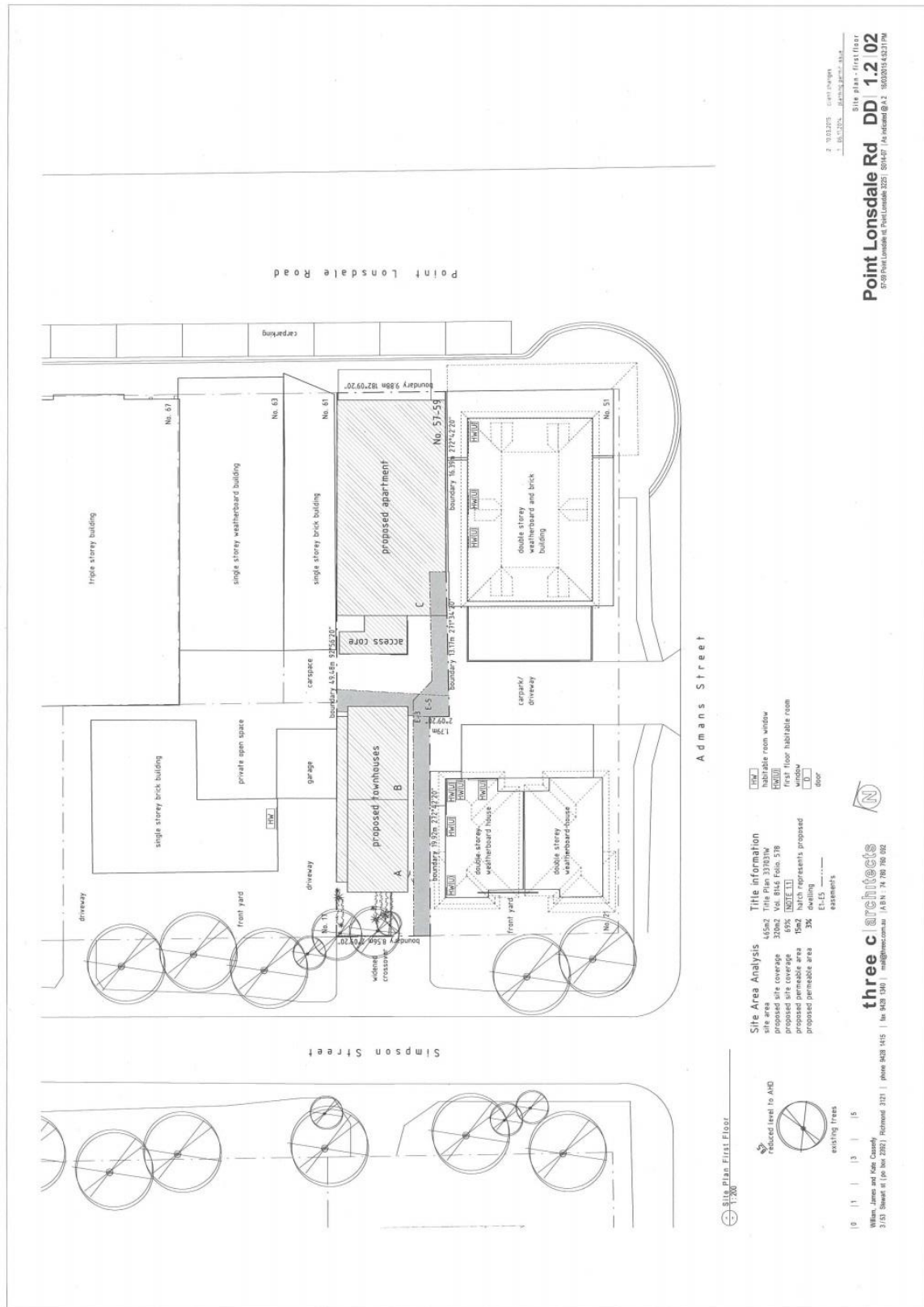
development, so as to positively influence the sustainability of buildings.	
9.1 Exceeding best practice standards	
9.2 Unique or new elements	Does not exceed best practice x
9.3 Passive design approach	x
9.4 Local climate conditions	Natural ventilation, double glazing, screening, and eaves reduced the heating and cooling loads the building receives. ✓
10 Construction and building Management	Screening of windows to prevent harsh summer sun. Double glazing to prevent heat loss and gain. ✓
Objective To encourage a holistic and integrated design and construction process and ongoing high performance. Examples of design decisions: Building Users Guide that explains a building's sustainable design principles Preparation of operation Environmental Management Plan Contractor has used ISO 14001 (environmental management) accreditation.	
10.1 Tuning of building systems	n/a
10.2 Building Users Guide	n/a
10.3 Operational Environmental Management plan	n/a
10.4 Environmental credentials	n/a

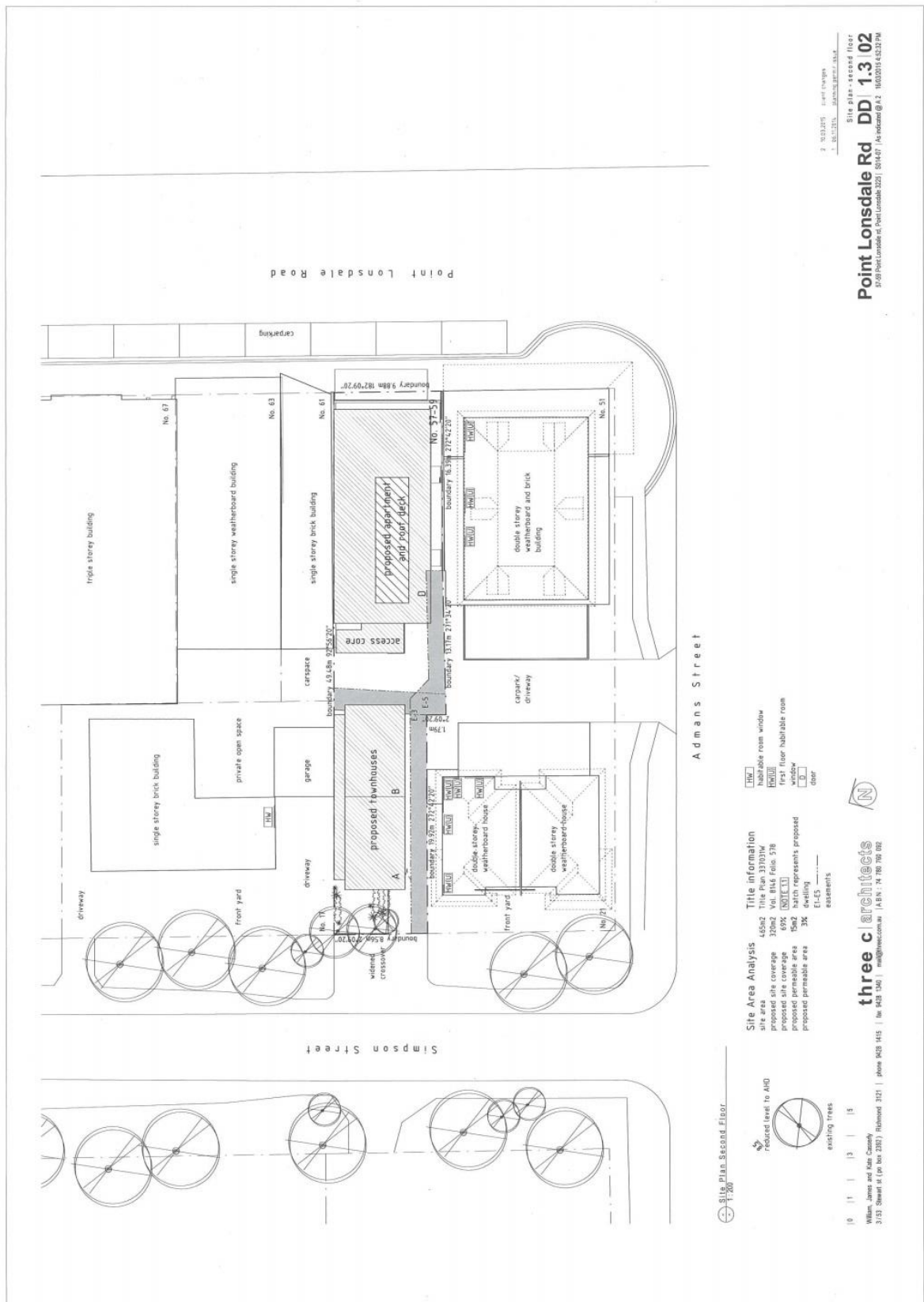


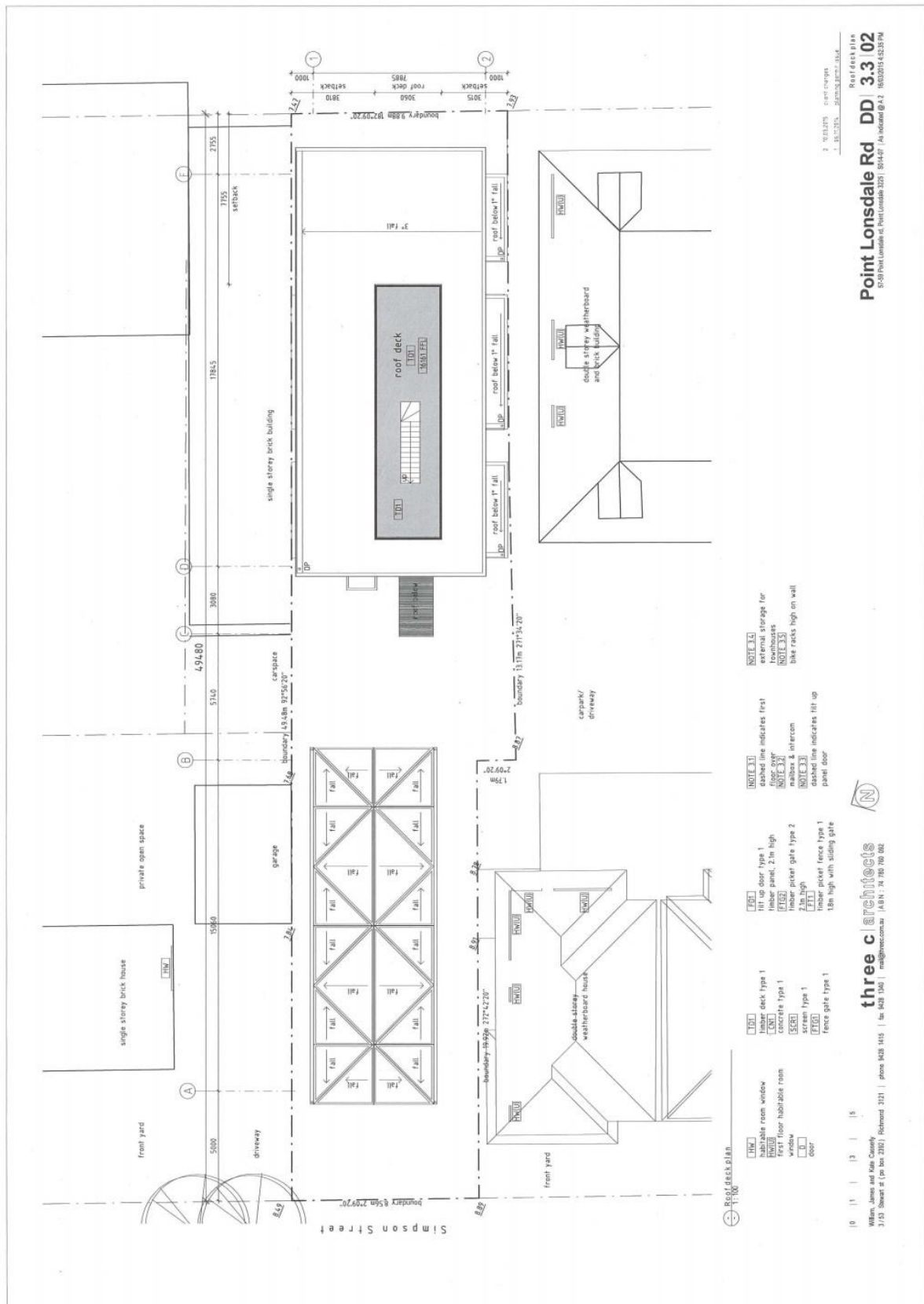
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6.2 Bike facilities	n/a	x
6.3 Public transport	The sites location is close to public transport. (bus)	x
6.4 Car share	n/a	x
6.5 Reducing onsite car parking	Reducing onsite car parking can save construction costs as either less space is required, or more space can be used for other purposes (i.e. bicycle parking and storage). In areas with readily accessible public transport and bike routes, Council will consider reducing the minimum number of car parking spaces required. This is provided that a Green Travel Plan is in place and that sufficient provision for alternative transport modes, such as bike parking and car sharing facilities, has been provided. Contact Council to discuss further details and refer to section 'What is a Green Travel Plan?' on the last page of this fact sheet.	x
7.0 Waste Management	Objective To ensure waste avoidance reuse and recycling during the construction and operation stages of development. General waste, recycling and green waste The new works shall recycle and operate waste in accordance with the Borough of Queenscliffe Space has been provided. ✓	
7.1		
7.2 Waste management plan	n/a	
7.3 Construction waste management plan		
8.0 Urban ecology	Objective To protect and enhance biodiversity and to encourage the planting of indigenous vegetation. Landscaped areas The economic benefits of having trees in the proximity to buildings can be both direct and indirect. Air-conditioning costs for cooling can be up to 50 percent lower in a tree shaded home as the home is provided with effective shading of windows and walls. Trees increase in value from the time they are planted until they mature. The housing market acknowledges that landscaped homes are more valuable than non-landscaped homes. The savings in energy costs and the increase in property value directly benefit each home buyer.	
8.1		
8.2 Native vegetation	Existing dense trees are located in Simpson St properties; therefore the proposed dwelling will take advantage of the trees location providing a strong visual relation between ground and first floors, and outdoors. In addition these trees will be able to control the chilling effects of winter winds. ✓	x
9.0 Innovation		
Objective	To encourage innovative technology, design and processes in all	

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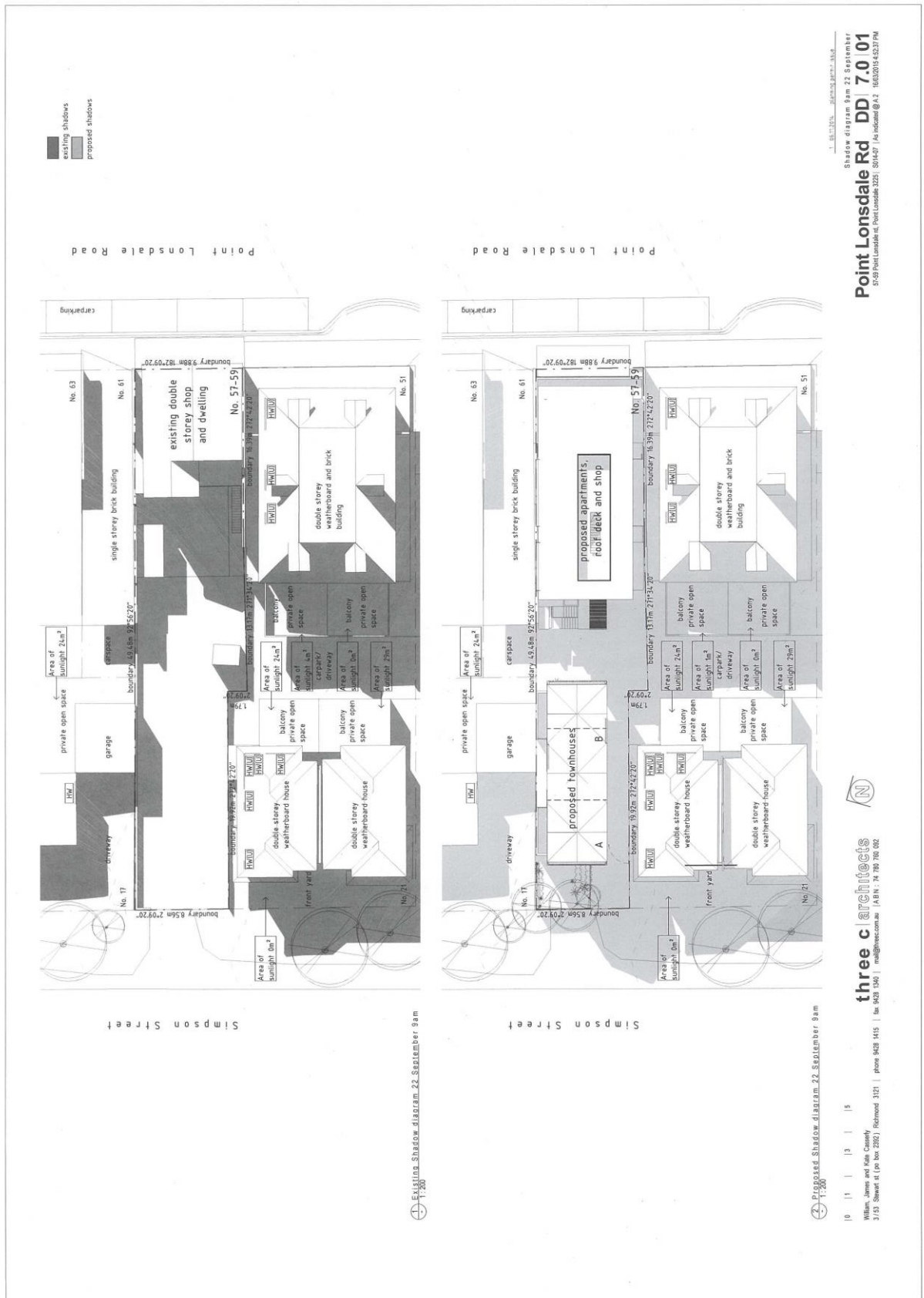


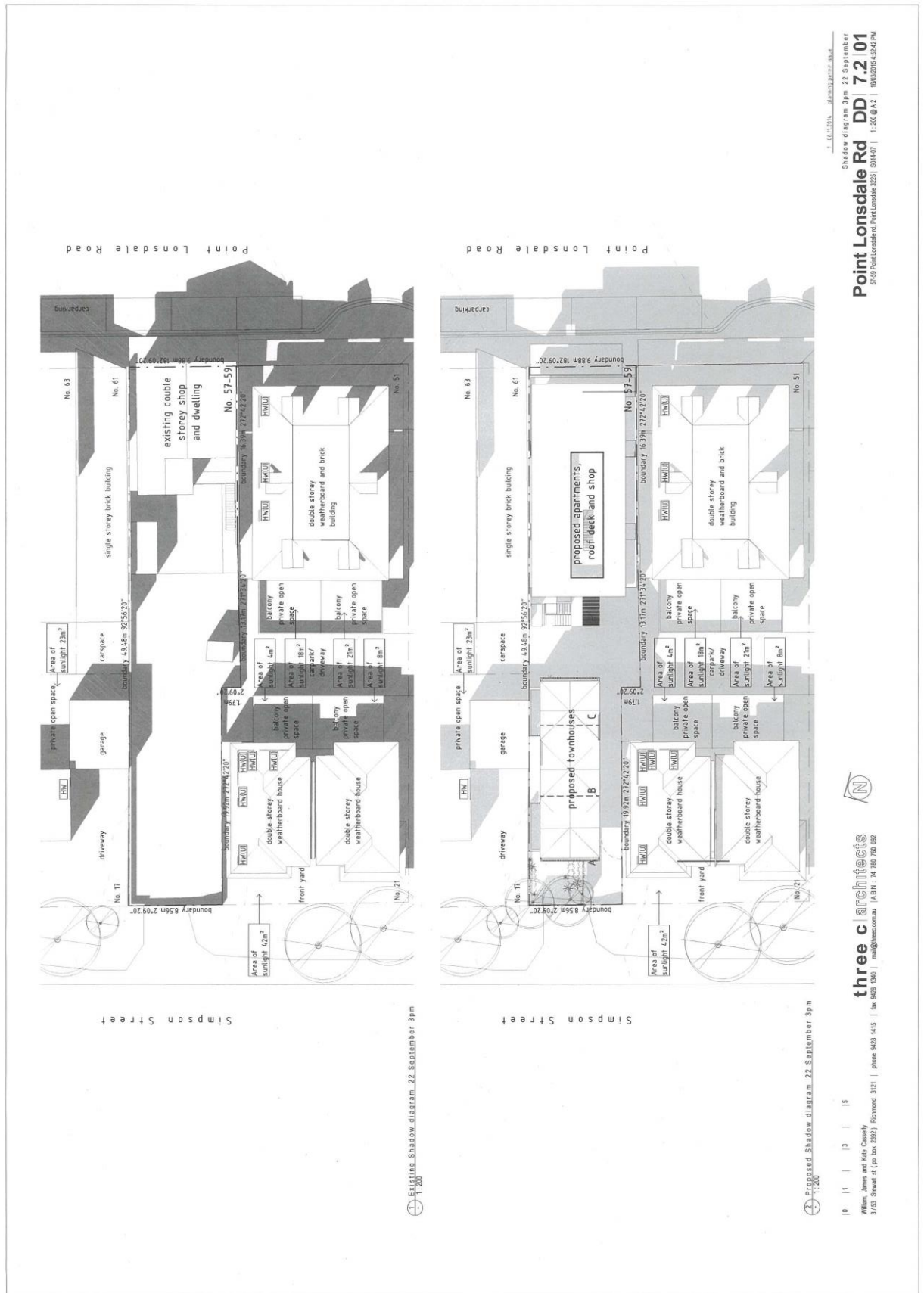


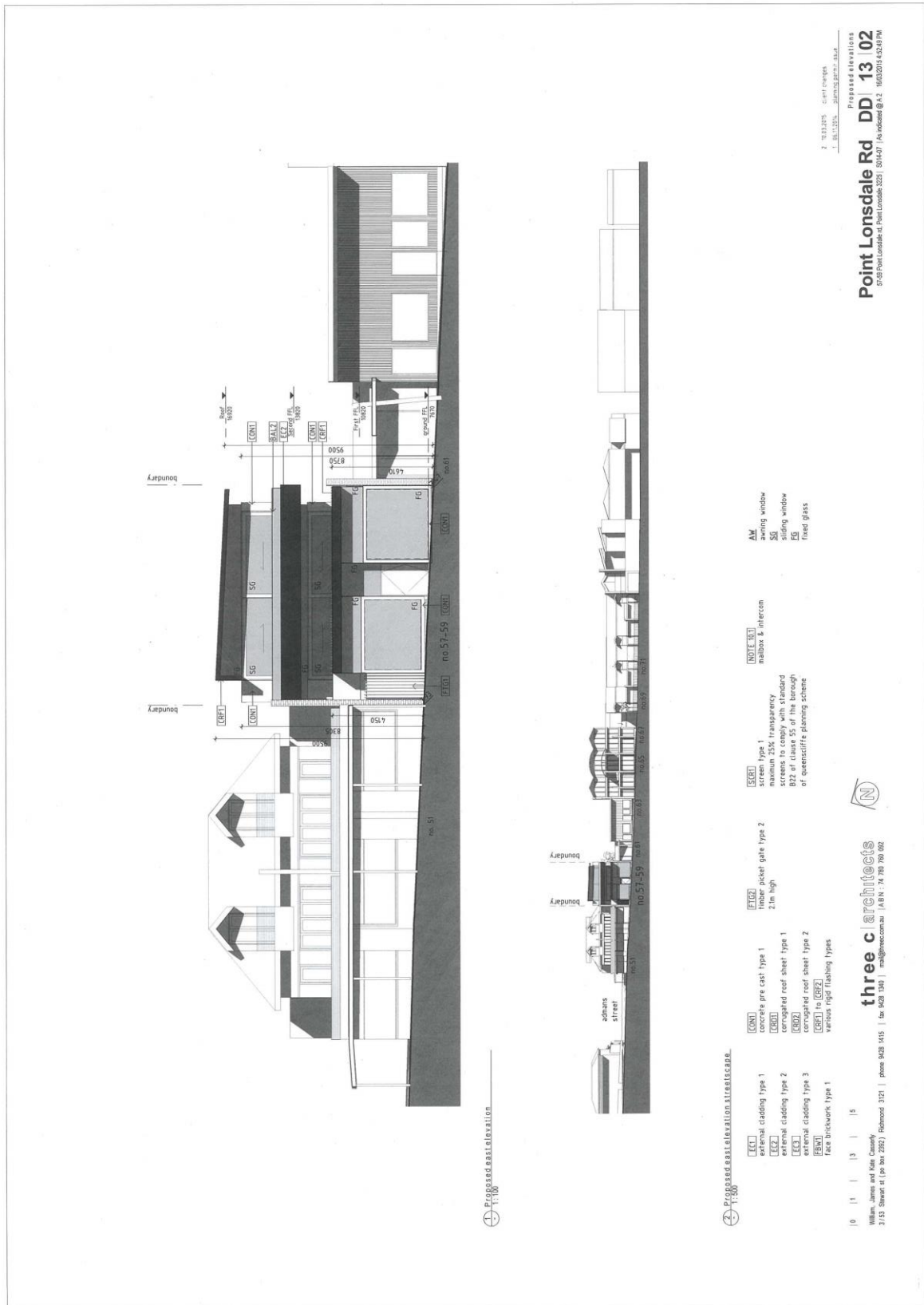
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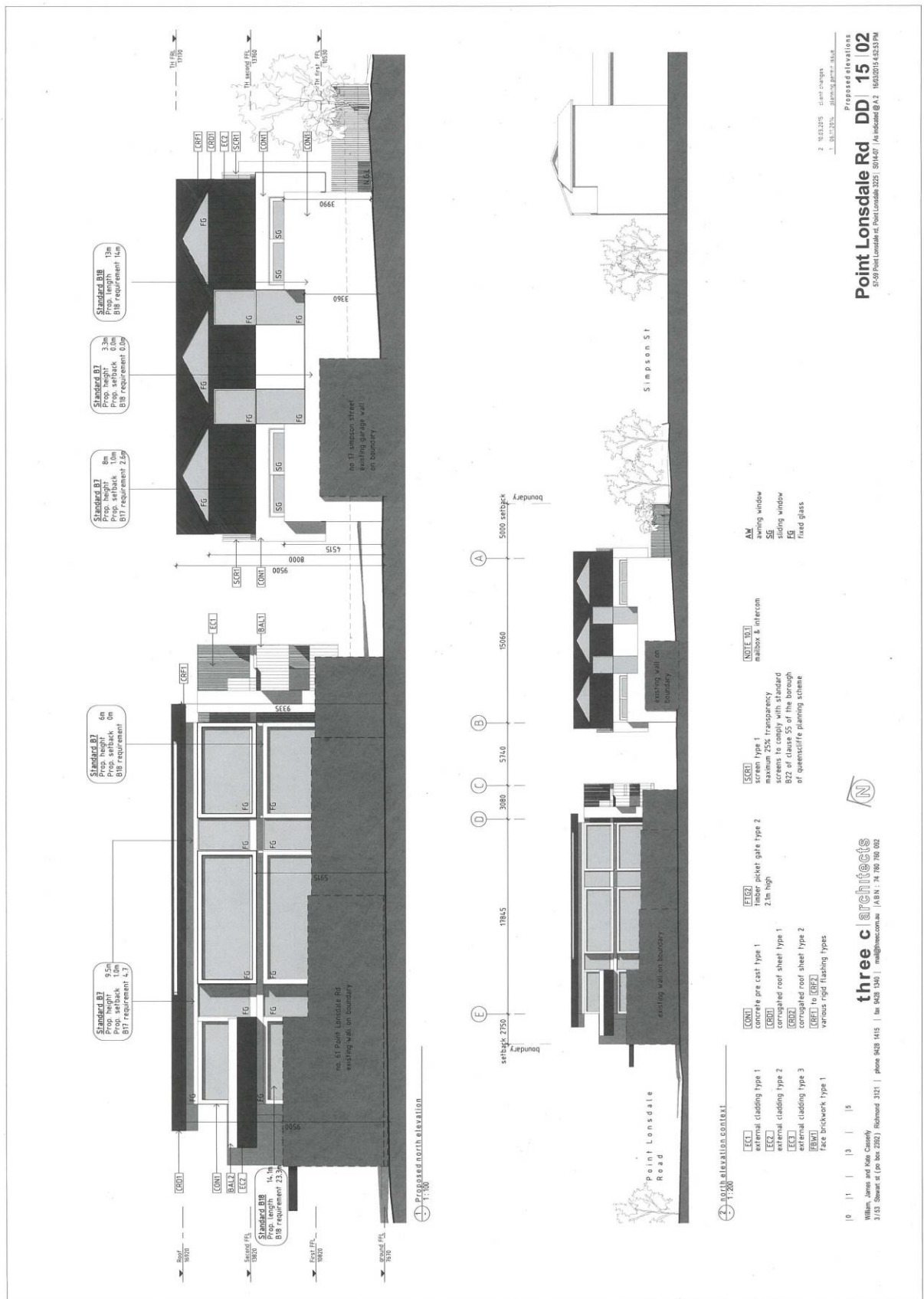
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William, James and Kelle Cassey











6. APPENDIX 2-(CONFIDENTIAL)-SUBMISSIONS,57-59 Point Lonsdale Road, Point Lonsdale

Provided to Councillors under separate cover



7. CLOSE OF MEETING